



**INDIAN WELLS COUNTRY CLUB COMMUNITY
FIRE ACCESS MAINTENANCE DISTRICT (FAMD) NO. 1**

**BOARD OF DIRECTORS MEETING
May 14, 2026
10:00 A.M.**

This Meeting will be held In-Person at
The City of Indian Wells City Council Chambers
44950 El Dorado Drive Indian Wells, CA



FAMD #1
Fire Access Maintenance District
An Agency of the City of Indian Wells
Indian Wells Country Club Community

BOARD OF DIRECTORS 2026

Director	Term Ends
Name: Kurt Yeager (Chairperson) Phone: (949) 632-6157 Email: ekurty01@gmail.com	February 28, 2029
Name: Bruce Morgan (Vice Chair) Phone: (406) 370-1224 Email: bkmzenmaster@gmail.com	February 28, 2029
Name: Steve Nozet (Secretary) Phone: (925) 698-4632 Email: nozetsteve@gmail.com	February 28, 2029
Name: Tony Trocino (Director) Phone: (310) 963-3428 Email: trotony7@gmail.com	February 28, 2030
Name: Mark Butier Jr. (Director) Phone: Email: jrbutier@butier.com	February 28, 2030
Name: Dennis Coker (IWCC Rep) Phone: (760) 345-2561 Email: Dennis.Coker@clubcorp.com	Appointed (No Term)



FIRE ACCESS MAINTENANCE DISTRICT NO. 1 INDIAN WELLS COUNTRY CLUB COMMUNITY

Board of Directors Meeting Agenda Thursday May 14, 2026 at 10:00 A.M.

**Board will Meet in the City Council Chamber of the
City of Indian Wells at 44950 El Dorado Indian Wells CA**

1. CALL TO ORDER OF THE FAMD, ROLL CALL

- Chair - Kurt Yeager
- Vice Chair – Bruce Morgan
- Secretary – Steve Nozet
- Director - Tony Trocino
- Director – Mark Butier
- IWCC Representative – Dennis Coker

2. PLEDGE OF ALLEGIANCE

3. APPROVAL the AGENDA

May 14, 2026

Page(s) 4-5

4. PUBLIC COMMENTS

All persons wishing to address the FAMD Board will be called on during this section of the meeting by the Chairman. At the appropriate time, please come forward to the podium and state your name for the record. Speakers are limited to three minutes. Parties are encouraged to submit their comments in writing with any attachments or exhibits they wish for the FAMD Board to review, preferably 24 hours prior to the meeting. Speakers can then use their three-minutes to summarize the key points of their comments. Please note that you may address the FAMD Board on an agenda item at the time it is discussed, but only after being recognized by the Chairman.

Under the Brown Act, the FAMD Board should not take action on or discuss matters raised during the public comment portion of the agenda which are not listed on the agenda. FAMD Board Members may refer such matters to staff for information or to be placed on a subsequent agenda for consideration. Notwithstanding the foregoing, FAMD Board Members and staff may briefly respond to statements made or questions posed during public comment, if such responses do not constitute any deliberation.

5. CONSENT CALENDAR

All matters listed on the Consent Calendar are routine and will be enacted by one vote. There will be no separate discussion of these items unless members of the FAMD Board or audience request that specific items be removed from the Consent Calendar for separate discussion and action. Financial matters will be indicated as budgeted or non-budgeted below.

- a. Minutes; April 9, 2025
- b. Financials; April 2025

Page(s) 6-7

Page(s) 8-9

6. SECURITY REPORT

- a. May 2026 Security Report

Page(s) 11-13



7. OLD BUSINESS

- a. Gate Access – Update on Challenges

Page(s) 15

8. NEW BUSINESS

- a. 2026/27 Pavement Projects
- b. Landscaping Committee
- c. Perone – Reimbursement request
- d. Radio System

Page(s) 17-71

Page(s) 72-73

Page(s) Verbal

Page(s) Verbal

9. FAMD DISTRICT MANAGERS REPORT

- a. Road Projects – Quail Run and Manitou
- b. Striping – May 26th thru May 29th
- c. Banking Update

10. BOARD MEMBERS COMMENTS

11. ANNOUNCEMENTS

The next Board of Directors meeting is scheduled for June 11, 2026.

12. ADJOURNMENT

**INDIAN WELLS COUNTRY CLUB COMMUNITY
FIRE ACCESS MAINTENANCE DISTRICT NO. 1
BOARD OF DIRECTORS OPEN MEETING**

IN COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT, IF YOU NEED SPECIAL ASSISTANCE TO PARTICIPATE IN THIS MEETING, PLEASE CONTACT THE SENIOR BUILDING INSPECTOR OR THE RISK MANAGER AT (760) 346-2489. A 48-HOUR NOTIFICATION PRIOR TO THE MEETING WILL ENABLE THE CITY TO MAKE REASONABLE ARRANGEMENTS TO ENSURE ACCESSIBILITY TO THIS MEETING (128 CFR 35.102.35.104 ADA TITLE III).

AFFIDAVIT OF POSTING

I, Angelica Avila, certify that on May 4, 2026, I caused to be posted a notice of a FAMD #1 Board Meeting to be held on Thursday May 7, 2026, at 10:00 A.M., in person in the City Council Chamber's.



BOARD OF DIRECTORS MEETING

MINUTES **April 9, 2026**

1. CALL TO ORDER

Chair Kurt Yeager called meeting to order at 10:03 A.M.

ROLL CALL

PRESENT: Chair - Kurt Yeager
Secretary – Steve Nozet
Director - Tony Trocino
Director – Bruce Morgan

ABSENT: Director Mark Butier
IWCC Representative – Dennis Coker

ALSO, PRESENT: District Manager Scott Matas (Desert Resort Management/ Associa),
Dina Purvis (City of Indian Wells, Public Works Director),
Paul Stotesbury (Director of Security, Allied Universal),
Michael Nelson (Account Manager, Allied Universal)

2. PLEDGE OF ALLEGIANCE

3. APPROVAL OF THE FINAL AGENDA

A motion was made by Director Tony Trocino and a 2nd by Director Steve Nozet to approve the agenda for the April 9, 2026. Motion carried 4/0.

4. PUBLIC COMMENTS

One member of the community attended the meeting.

Teresa Marley – Sandpiper #11 – Her comments were focused on Sandpiper 11 street striping and pedestrian safety at Sandpiper and Pueblo.

CONSENT CALENDAR

- a. Minutes; March 12, 2026
- b. Financials; March 2026

A motion was made by Director Steve Nozet and a 2nd by Director Tony Trocino to approve the Consent Calendar. Motion carried 4/0.

5. SECURITY REPORT

- a. March 2026 Security Report

Director of Security (DOS) Paul Stotesbury presented the monthly security report and answered questions.

Note: The Board of Directors have received an unusual number of concerns regarding customer service at the security gates. The account manager Michael Nelson, Director of Security Paul Stotesbury and the District Manager updated the Board on the action plan on customer service training.

6. OLD BUSINESS

a. Security Services Ad Hoc Committee Update

The Chair thanked the committee and disbanded.

7. NEW BUSINESS

a. Landscaping Proposals

The District Manager presented multiple landscaping proposals to freshen up the Manitou entry and remove a dead tree.

A motion was made by Director Steve Nozet and seconded by Director Bruce Morgan to approve an amount not to exceed \$8,000.00 for landscaping refresh and removal of a dead tree at the Manitou gate. The motion carried by a vote of 4/0.

8. DISTRICT MANAGER REPORT

The District Manager reported and answered questions regarding Allied Universal contract, road projects, striping, gate access, water features, accounting transition and transponders.

9. BOARD MEMBER COMMENTS

- Chair Kurt Yeager – Assigned a new Landscaping Committee and to have the District Manager look at the exposed rebar on the bridges.
- Steve Nozet – reported he will be out of the country June through October and requested the district manager look into gate access app notification to the community on how to use.

10. ANNOUCEMENTS

The next regularly scheduled meeting of the Fire Access Maintenance District Board of Directors will be held at 10:00 A.M., on May 14, 2026, in person at the City of Indian Wells council chambers.

11. ADJOURNMENT

There being no more business the meeting was adjourned by Chair Kurt Yeager at 11:20 A.M.

Respectfully Submitted, Scott Matas, District Manager

Attested to: _____ Date: _____
Steve Nozet, FAMD Secretary

FAMD Financial Monthly Report	
Month: April 2026	
Monthly Income	\$168,885.08
Monthly Expenses	\$100,691.96
Available Budget - Expenses	\$523,023.49
Available Encumbered - Expenses	\$437.75
Available Total Budgeted Expenses	\$523,461.24
Operations Budget (Approved)	\$1,620,000.00
Capital Budget (Actual)	\$212,122.00
Total Projected Expenditures	\$2,141,586.00
Assessed & Tax Collection Revenues	\$2,329,900.00
FY Projected Balance	\$709,900.00
Current Fund Balance	\$1,246,664.71
Project Fund Balance End of FY 2025/26	\$1,175,175.59

Projected FY 2025/26	
Revenue	\$ 2,329,900.00
Expenditures	\$ 1,620,000.00
Total	\$ 709,900.00

Actual to YTD - December 2025	
Revenue	\$ 1,432,560.17
Expenditure	\$ 1,096,538.76
Total	\$ 336,021.41

YEAR-TO-DATE BUDGET REPORT

FOR 2026 10

	ORIGINAL APPROP	REVISED BUDGET	YTD ACTUAL	MTD ACTUAL	ENCUMBRANCES	AVAILABLE BUDGET	PCT USE/COL
209 F.A.M.D. #1							
2090000 F.A.M.D. #1							
2090000 411100 Current Secured Pro	-431,500	-431,500	-380,269.56	-168,885.08	.00	-51,230.44	88.1%*
2090000 411200 Current Unsecured P	-10,800	-10,800	-10,727.43	.00	.00	-72.57	99.3%*
2090000 411300 Prior Year Property	-4,100	-4,100	-2,126.45	.00	.00	-1,973.55	51.9%*
2090000 411400 Supplemental Roll P	-7,800	-7,800	-2,369.55	.00	.00	-5,430.45	30.4%*
2090000 421100 Fire Access Maint.	-1,785,700	-1,785,700	-1,019,642.08	.00	.00	-766,057.92	57.1%*
2090000 451100 Investment Earnings	-68,400	-68,400	-13,801.00	.00	.00	-54,599.00	20.2%*
2090000 461500 Homeowner's Prop. T	-18,500	-18,500	-699.10	.00	.00	-17,800.90	3.8%*
2090000 486300 Miscellaneous Reven	-3,100	-3,100	-2,925.00	-600.00	.00	-175.00	94.4%*
TOTAL F.A.M.D. #1	-2,329,900	-2,329,900	-1,432,560.17	-169,485.08	.00	-897,339.83	61.5%
2098601 F.A.M.D. Program							
2098601 531000 Professional Servis	156,100	156,100	85,189.94	7,714.14	437.75	70,472.31	54.9%
2098601 533000 Other Contracted Se	1,102,500	1,102,500	820,849.75	86,791.36	.00	281,650.25	74.5%
2098601 542000 Utilities	37,000	37,000	16,838.92	606.13	.00	20,161.08	45.5%
2098601 544000 Building & Grounds	157,000	155,900	53,572.69	5,219.76	.00	102,327.31	34.4%
2098601 545000 Infrastructure	125,000	127,000	95,031.72	.00	.00	31,968.28	74.8%
2098601 552000 Communications	20,100	20,100	14,109.99	360.57	.00	5,990.01	70.2%
2098601 561000 Office Expense	3,600	3,600	.00	.00	.00	3,600.00	.0%
2098601 563000 Operating Materials	1,700	2,800	945.75	.00	.00	1,854.25	33.8%
2098601 591600 Indirect Costs	15,000	15,000	10,000.00	.00	.00	5,000.00	66.7%
TOTAL F.A.M.D. Program	1,618,000	1,620,000	1,096,538.76	100,691.96	437.75	523,023.49	67.7%
2099929 FAMD Capita							
2099929 673100 Construction Contra	0	212,122	.00	.00	.00	212,122.00	.0%
TOTAL FAMD Capital	0	212,122	.00	.00	.00	212,122.00	.0%
TOTAL REVENUES	-2,329,900	-2,329,900	-1,432,560.17	-169,485.08	.00	-897,339.83	
TOTAL EXPENSES	1,618,000	1,832,122	1,096,538.76	100,691.96	437.75	735,145.49	
GRAND TOTAL	-711,900	-497,778	-336,021.41	-68,793.12	437.75	-162,194.34	67.4%

** END OF REPORT - Generated by Amy Da'llosta **

BEGINNING FUND BALANCE 7/1/2025 910,643.30
CHANGE IN FUND BAL - NET OF REVENUES/EXPENSES 336,021.41
ENDING FUND BALANCE 4/30/2026 1,246,664.71

FAMD #1

INDIAN WELLS COUNTRY CLUB COMMUNITY

SECURITY REPORT

Paul A. Stotesbury
Site Security Director
April, 2026

- April 8, 2026 Cota Way, Animal Control called and cited loose dogs
- April 10-13, 2026 Coachella Festival quiet no problems
- April 17-19, 2026 Coachella Festival quiet no problems
- April 24-26, 2026 Stagecoach Festival quiet no problems
- April 11, 2026 Club Gate, property damage, vendor hit overhead, slight damage, photos/ report to DRM
- April 15, 2026 Pawnee, RSO (2) responded looking for subject, located on Pawnee, took into custody, no further information

FIRE DEPARTMENT/ PARAMEDICS

RESPONSES - 15	TRANSPORTS – 10
	<u>RSO</u>
ROUTINE PATROL – 36	CALLS FOR SERVICE - 2
OPEN GARAGE DOOR – 56	OPEN OTHER DOORS – 1
CLUB HOUSE DOORS – 23	TRANSPONDERS INSTALLED – 71

Indian Wells Country Club FAMD #1
Security Staffing Report

Employee Name	Length of Service at Allied Universal in Months	Length of Service at IWCCC in Months	Position	Scheduled Work Days	Scheduled Work Hours	Total Scheduled Hours/Day	Total Scheduled Hours/Week
1 Casarez, Jose	100	97	Security Officer	Sun/Thurs	2pm-10pm	8	40
2 Garcia, Dominick	6	6	Security Supervisor	Tues/Sat	2pm-10pm	8	40
3 Gonzalez, Gonzalo	47	29	Security Officer	Sun/Tues/Wed	2pm-10pm	8	24
4 Gray, Ken	43	43	Security Officer	Mon/Tues	2pm-10pm	8	16
5 Hernandez, Josue	21	21	Security Officer	Thurs/Sun	10pm-6am	8	40
6 Hosamane, Mahinder	151	201	Security Supervisor	Sun/Thurs	10pm-6am	8	40
7 Hullaby, Leticia	7	7	Security Officer	Wed/Sun	2pm-10pm	8	40
8 Lara, Marvin	154	153	Security Senior Supervisor	Mon/Sat	6am - 2pm	8	40
9 Lizarraga, Jesus	15	14	Security Officer	Thurs/Mon	6am-2pm	8	40
10 Medina, Hazel	2	2	Security Officer	Sun/Thurs	2pm-10pm	8	40
11 Parsons, Caleb	16	15	Security Officer	Wed/Sun	2pm/10pm	8	40
12 Pennington, Sandra	44	45	Security Officer	Fri/Mon	10pm-6am	8	32
13 Penny, Max	50	50	Security Officer	Tues/Sat	2pm-10pm	8	40
14 Perez, Helen	28	37	Security Officer	Wed/Sun	2pm-10pm	8	40
15 Rivas, Adam	14	14	Security Officer	Sat/Wed	varieas	8	40
16 Sandoval, Andrea	108	85	Security Officer	Sat-Wed	6am-2pm	8	40
17							
18 Stotesbury, Paul	128	94	Site Security Supervisor	Mon- Fri	Varies	8	40
19 Part Time (Castaneda)			Security Officer		6am-2pm	8	16
TOTALS	954	913					648
AVERAGES	54.94	53.71					

Change Change

Stats for 2018

Event	Jan.	Feb	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Total
Access Denied	0	0	0	0	0	0	0	0	0	0	0	0	0
Alarm Checks-Security	1	1	1	1									4
Animal Control on site	0	0	0	1									1
Animal Issues	1	0	0	1									2
Burglaries	0	0	0	0									0
Thefts	0	0	0	0									0
Garage Doors Open	41	36	57	56									190
A Shift up to 1400 hrs	22	0	0	0									22
B Shift up to 2200 hrs	19	33	47	48									147
C shift after 2200 hrs	0	3	10	8									21
Doors Open (Other)	19	21	29	24									93
Golf Cart Issues	0	0	0	1									1
Homeowner Assist/Welfare Check	2	5	2	2									11
Landscape/Light Issues	0	2	0	1									3
Medical Response / Fire Dept.	19	16	12	15									62
Noise Issues	1	0	1	3									5
Other - Domestic Issue	0	0	0	0									0
Parking Issues	2	0	4	2									8
Public Utility Issue	0	1	0	0									1
Process Servers	6	3	1	3									13
Property Issues/Damage	2	7	4	4									17
Realtor Issues	0	0	0	0									0
Sheriff on Site	36	17	36	36									125
Alarm Response/Call In	3	3	4	11									21
Routine Patrol	33	14	32	25									90
Suspicious Events	0	1	6	0									7
Traffic/Street Issues	0	0	0	0									0
Vandalism	0	0	0	0									0
Vendor Issues	0	0	1	0									1
Water Issues	11	5	4	10									30

STAFF REPORT

DATE: May 12, 2026
TO: Honorable FAMD Board Members
FROM: FAMD District Manager
SUBJECT: Gate Access

SUMMARY:

A discussion was previously initiated at a Board meeting following comments from resident Reed Beidler regarding the early morning start times for golf course maintenance. The Board subsequently requested a review of the Security Post Orders, applicable City of Indian Wells ordinances, club policies, and practices at comparable clubs.

The Security Director previously reported that City ordinances permit earlier start times than those currently in question. Additionally, the District Manager confirmed that other clubs in the area, such as Toscana, begin golf course maintenance activities early in the morning.

The Security Post Orders provide the following guidance regarding club employees:

3.5.3 Country Club Employee Passes

Passes for employees are issued at the Club Drive gate. Country Club administration provides an updated list of all employees to the Site Security Manager on a monthly basis. Additions and deletions are communicated to Security upon hire or termination.

At no time have Security Post Orders directed staff to restrict employee access to the club during specific hours. Furthermore, the Club Director, who serves as a member of the Board, is available to address any operational questions regarding maintenance schedules and procedures.

Historically, the FAMD has not regulated employee access for the club, and it is recommended that this practice continue.

ATTACHMENTS

None

FISCAL IMPACT:

None

RECOMMENDATIONS

Discuss and give direction.

STAFF REPORT

DATE: May 12, 2026
TO: Honorable FAMD Board Members
FROM: FAMD District Manager
SUBJECT: Road Rehab Project Recommendation

SUMMARY:

The Board of Directors requested a recommended capital expenditure project list for FY 2026/27. The proposed project list consists of the remaining streets identified as being in “fair” condition per the Pavement Management Plan. While the plan includes annual recommendations, the Board previously expressed a preference to prioritize the lowest-rated (worst condition) streets.

The estimated capital cost for FY 2026/27, based on the plan, is \$197,167. However, the total estimated cost to address all remaining “fair” condition streets is approximately \$700,000. Please note that two streets were not included in the original estimates forecast maintenance report.

For cash flow purposes, it is recommended that total expenditures not exceed \$500,000.

Additionally, an alternative approach may be considered for certain streets, such as crack fill and seal coat or grind and overlay, in place of slurry sealing.

ATTACHMENTS

Exhibit “A”
Exhibit “B”
MAP
Pavement Management Plan

FISCAL IMPACT:

TBD

RECOMMENDATIONS

Discuss and give direction.

Sorted by PCI Order (0-100)

Indian Wells Country Club - FAMD #1
Pavement Condition Index (PCI) Report - All Streets

BranchID	SecID	Name	From	To	Surface Type	Rank	L	W	Area	Insp. Date	PCI	PCI Pct	Climate	PCI Pct	Load	PCI Pct	Other	Work Type	Work Date
40	700	QUAIL RUN LN	IROQUOIS DR	BRIDGE	AC	E	488	36	17,318	6/14/24	55	75		24	1		New Construction - Initial	2/1/00	
13	160	CLUB TERRACE DR	IROQUOIS DR	DESI DR	AC	E	524	28	14,812	6/13/24	64	47		52	1		Surface Treatment - Slurry Seal	11/1/23	
49	910	SKY MESA LN	BOX MOUNTAIN RD	COTA WAY	AC	E	673	26	17,758	6/14/24	66	53		47	0		New Construction - Initial	2/1/00	
31	520	MANITOU DR	SIoux DR	IROQUOIS DR	AC	E	721	32	22,822	6/13/24	68	71		29	0		New Construction - Initial	2/1/00	
54	970	TRIBECCA ST	BOX MOUNTAIN RD	MANITOU DR	AC	E	786	32	24,502	6/14/24	68	59		41	0		New Construction - Initial	2/1/00	
7	80	BOX RIDGE PL	COTA WAY	CDS	AC	E	448	28	13,824	6/14/24	69	55		45	0		New Construction - Initial	2/1/00	
34	590	ONTVEROS CT	EPINAZO ST	CDS	AC	E	239	30	9,120	6/14/24	70	70		30	0		New Construction - Initial	2/1/00	
9	100	CAYUGA LN	DELGADO DR	CDS	AC	E	267	28	8,926	6/14/24	70	60		40	0		Surface Treatment - Slurry Seal	2/1/00	
30	490	MALLORCA LN	COTA WAY	BOX MOUNTAIN LN	AC	E	728	28	21,064	6/14/24	70	57		43	0		New Construction - Initial	2/1/00	
22	290	ESPINAZO ST	TRIBECCA ST	CDS	AC	E	279	32	10,208	6/14/24	71	78		22	0		New Construction - Initial	2/1/00	
52	940	TETON LN	DELGADO DR	CDS	AC	E	221	28	7,538	6/14/24	71	81		15	4		Surface Treatment - Slurry Seal	11/1/23	
55	980	VAIDYA CT	TRIBECCA ST	CDS	AC	E	331	31	11,941	6/14/24	71	83		17	0		New Construction - Initial	2/1/00	
22	300	ESPINAZO ST	TRIBECCA ST	ONTVEROS CT	AC	E	351	32	11,232	6/14/24	72	78		22	0		New Construction - Initial	2/1/00	
16	200	COTA WAY	SKY MESA LN	MALLORCA LN	AC	E	451	28	13,878	6/14/24	72	61		39	0		New Construction - Initial	2/1/00	
1	20	APPIAN WAY	SANDPIPER DR	CDS	AC	E	502	32	17,844	6/13/24	73	67		33	0		New Construction - Initial	1/1/00	
44	800	SANDPIPER DR	CLUB DR	HOPI RD	AC	E	1,023	28	28,644	6/13/24	73	58		42	0		Surface Treatment - Slurry Seal	11/1/23	
13	170	CLUB TERRACE DR	DESI DR	CDS	AC	E	975	30	30,900	6/13/24	73	79		21	0		Surface Treatment - Slurry Seal	11/1/23	
25	370	INCA DR	AZTEC DR	TOMAHAWK RUN	AC	E	1,245	26	32,370	6/13/24	74	61		39	0		New Construction - Initial	2/1/00	
47	890	SHASTA LN	DELGADO DR	CDS	AC	E	304	28	9,962	6/14/24	74	85		15	0		Surface Treatment - Slurry Seal	11/1/23	
44	810	SANDPIPER DR	HOPI RD	PUEBLO RD	AC	E	860	28	24,080	6/13/24	74	65		35	0		Surface Treatment - Slurry Seal	11/1/23	
27	410	IROQUOIS DR	MANITOU DR	APPIAN WAY	AC	E	1,154	46	53,084	6/13/24	75	88		12	0		New Construction - Initial	2/1/00	

Condition	PCI Range	Section Miles	% of Network
Very Good	86-100	4.8	38%
Good	75-85	5.7	45%
Fair	60-74	2.1	17%
Poor	41-59	0.1	1%
Very Poor	0-40	0.0	0%
		12.7	

Indian Wells Country Club - FAMD #1
Forecast Maintenance / Rehabilitation (FMR) Report - FY 2024-2029

Sorted by FY, Name Order (A-Z)

FY	BranchID	SecID	Name	From	To	Surface Type	Rank	L	W	Area	PCI	PCI Pct Climate	PCI Pct Load	PCI Pct Other	Maint. Type	Total \$	Work Type	Work Date	
2024-25	The District does not have slurry seal and overlay projects scheduled for FY 2024-25																		
2025-26	1	10	APPAIN WAY	IROQUOIS DR	SANDPIPER DR	AC	E	1,150	36	41,080	77	81	19	0	Type1 Slurry Seal	\$17,254	New Construction - Initial	1/1/00	
2025-26	1	20	APPAIN WAY	SANDPIPER DR	CDS	AC	E	502	32	17,844	73	67	33	0	Type1 Slurry Seal	\$7,494	New Construction - Initial	1/1/00	
2025-26	7	80	BOX RIDGE PL	COTA WAY	CDS	AC	E	448	28	13,824	69	55	45	0	AC Grind-Overlay	\$54,605	New Construction - Initial	1/1/00	
2025-26	30	490	MALLORCA LN	COTA WAY	BOX MOUNTAIN LN	AC	E	728	28	21,064	70	57	43	0	AC Grind-Overlay	\$83,203	New Construction - Initial	2/1/00	
2025-26	44	830	SANDPIPER DR	MANITOU DR	BOX MOUNTAIN RD	AC	E	772	45	34,740	76	92	8	0	Type1 Slurry Seal	\$14,591	New Construction - Initial	2/1/00	
2025-26	44	840	SANDPIPER DR	BOX MOUNTAIN RD	APPAIN WAY	AC	E	930	45	41,850	77	80	20	0	Type1 Slurry Seal	\$17,577	New Construction - Initial	2/1/00	
2025-26	44	850	SANDPIPER DR	PUEBLO RD	HOP RD	AC	E	860	22	18,920	81	100	0	0	Type1 Slurry Seal	\$7,946	New Construction - Initial	2/1/00	
2025-26	44	860	SANDPIPER DR	HOP RD	CLUB DR	AC	E	1,023	22	22,506	80	97	0	3	Type1 Slurry Seal	\$9,453	New Construction - Initial	2/1/00	
2026-27	2	30	AZTEC DR	COMANCHE LN	INCA DR	AC	E	419	35	13,465	79	80	20	0	Type1 Slurry Seal	\$5,790	New Construction - Initial	1/1/00	
2026-27	14	180	COMANCHE LN	MANITOU DR	CDS	AC	E	918	36	33,048	78	73	27	0	Type1 Slurry Seal	\$14,211	New Construction - Initial	1/1/00	
2026-27	25	370	INCA DR	AZTEC DR	TOMAHAWK RUN	AC	E	1,245	26	32,370	74	61	39	0	Type1 Slurry Seal	\$13,919	New Construction - Initial	2/1/00	
2026-27	31	520	MANITOU DR	SIQUA DR	IROQUOIS DR	AC	E	721	32	22,622	68	71	29	0	AC Grind-Overlay	\$93,798	New Construction - Initial	2/1/00	
2026-27	34	590	ONTVEROS CT	EPINAZO ST	CDS	AC	E	239	30	9,120	70	70	30	0	AC Grind-Overlay	\$37,483	New Construction - Initial	2/1/00	
2026-27	53	950	TOMOHAWK RUN	INCA DR	AZTEC DR	AC	E	1,541	32	49,312	82	86	14	0	Type1 Slurry Seal	\$21,204	New Construction - Initial	2/1/00	
2026-27	53	960	TOMOHAWK RUN	AZTEC DR	CDS	AC	E	400	32	13,400	77	100	0	0	Type1 Slurry Seal	\$5,762	New Construction - Initial	2/1/00	
2027-28	6	70	BOX MOUNTAIN RD	SANDPIPER DR	SKY MESA LN	AC	E	731	39	30,749	79	54	46	0	Type1 Slurry Seal	\$19,167	New Construction - Initial	1/1/00	
2027-28	13	160	CLUB TERRACE DR	IROQUOIS DR	DESI DR	AC	E	524	28	14,612	64	47	52	1	AC Grind-Overlay	\$63,247	Surface Treatment - Slurry Seal	11/1/23	
2027-28	22	290	EPINAZO ST	TRIBECCA ST	CDS	AC	E	279	32	10,208	71	78	22	0	Type1 Slurry Seal	\$4,594	New Construction - Initial	2/1/00	
2027-28	22	300	EPINAZO ST	TRIBECCA ST	ONTVEROS CT	AC	E	351	32	11,232	72	78	22	0	Type1 Slurry Seal	\$5,054	New Construction - Initial	2/1/00	
2027-28	31	530	MANITOU DR	IROQUOIS DR	SANDPIPER DR	AC	E	1,131	45	50,895	78	88	12	0	Type1 Slurry Seal	\$22,903	New Construction - Initial	2/1/00	
2027-28	31	540	MANITOU DR	SANDPIPER DR	COMANCHE LN	AC	E	847	45	36,115	76	68	24	8	Type1 Slurry Seal	\$17,152	New Construction - Initial	2/1/00	
2027-28	31	550	MANITOU DR	COMANCHE LN	GATE	AC	E	88	35	3,080	85	100	0	0	Type1 Slurry Seal	\$1,386	New Construction - Initial	2/1/00	
2027-28	31	560	MANITOU DR	GATE	COMANCHE LN	AC	E	88	33	2,904	85	100	0	0	Type1 Slurry Seal	\$1,207	New Construction - Initial	2/1/00	
2027-28	40	700	QUAIL RUN LN	IROQUOIS DR	BRIDGE	AC	E	488	36	17,318	55	75	24	1	AC Grind-Overlay	\$73,948	New Construction - Initial	2/1/00	
2027-28	49	910	SKY MESA LN	BOX MOUNTAIN RD	COTA WAY	AC	E	673	26	17,758	66	53	47	0	AC Grind-Overlay	\$75,827	New Construction - Initial	2/1/00	
2027-28	52	940	TETON LN	DELGADO DR	CDS	AC	E	221	28	7,538	71	81	15	4	AC Grind-Overlay	\$32,187	Surface Treatment - Slurry Seal	11/1/23	
2027-28	55	980	VAIDYA CT	TRIBECCA ST	CDS	AC	E	331	31	11,941	71	83	17	0	Type1 Slurry Seal	\$5,373	New Construction - Initial	2/1/00	
2028-29	8	90	CASAS DE MARIPOSA	TOMAHAWK RUN	CDS	AC	E	498	32	17,686	83	85	15	0	Type1 Slurry Seal	\$8,112	New Construction - Initial	1/1/00	
2028-29	13	170	CLUB TERRACE DR	DESI DR	CDS	AC	E	975	30	30,900	73	79	21	0	AC Grind-Overlay	\$35,960	Surface Treatment - Slurry Seal	11/1/23	
2028-29	15	190	CORTE PICCOLO	APPAIN WAY	CDS	AC	E	455	36	17,510	79	100	0	0	Type1 Slurry Seal	\$8,230	New Construction - Initial	2/1/00	
2028-29	16	200	COTA WAY	SKY MESA LN	MALLORCA LN	AC	E	451	28	13,878	72	61	39	0	Type1 Slurry Seal	\$6,523	New Construction - Initial	2/1/00	
2028-29	24	360	HOP RD	SANDPIPER DR	CDS	AC	E	223	30	8,540	77	100	0	0	Type1 Slurry Seal	\$4,014	New Construction - Initial	2/1/00	
2028-29	27	410	IROQUOIS DR	MANITOU DR	APPAIN WAY	AC	E	1,154	46	53,084	75	88	12	0	Type1 Slurry Seal	\$24,949	New Construction - Initial	2/1/00	
2028-29	27	420	IROQUOIS DR	APPAIN WAY	QUAIL RUN LN	AC	E	1,099	46	50,554	79	63	33	4	Type1 Slurry Seal	\$23,760	New Construction - Initial	2/1/00	
2028-29	27	430	IROQUOIS DR	QUAIL RUN LN	VIA VILLAGGIO	AC	E	717	46	32,982	81	98	0	2	Type1 Slurry Seal	\$15,502	New Construction - Initial	2/1/00	
2028-29	38	670	PIMA RD	SANDPIPER DR	CDS	AC	E	240	35	10,700	75	91	9	0	Type1 Slurry Seal	\$5,029	New Construction - Initial	2/1/00	
2028-29	44	800	SANDPIPER DR	CLUB DR	HOP RD	AC	E	1,023	28	28,644	73	58	42	0	AC Grind-Overlay	\$126,034	Surface Treatment - Slurry Seal	11/1/23	
2028-29	54	970	TRIBECCA ST	BOX MOUNTAIN RD	MANITOU DR	AC	E	786	32	24,502	68	59	41	0	AC Grind-Overlay	\$107,809	New Construction - Initial	2/1/00	
2028-29	56	990	VIA VENITO	VIA VILLAGGIO	IROQUOIS DR	AC	E	706	36	25,416	80	86	14	0	Type1 Slurry Seal	\$11,946	New Construction - Initial	2/1/00	
2028-29	57	1000	VIA VILLAGGIO	IROQUOIS DR (W)	IROQUOIS DR (E)	AC	E	1,809	36	66,104	75	78	19	3	Type1 Slurry Seal	\$31,069	New Construction - Initial	2/1/00	
2028-29	57	1010	VIA VILLAGGIO	IROQUOIS DR	CDS	AC	E	285	35	11,755	79	84	16	0	Type1 Slurry Seal	\$5,215	New Construction - Initial	2/1/00	
																Total 5-Yr Budget			
																\$1,235,756			

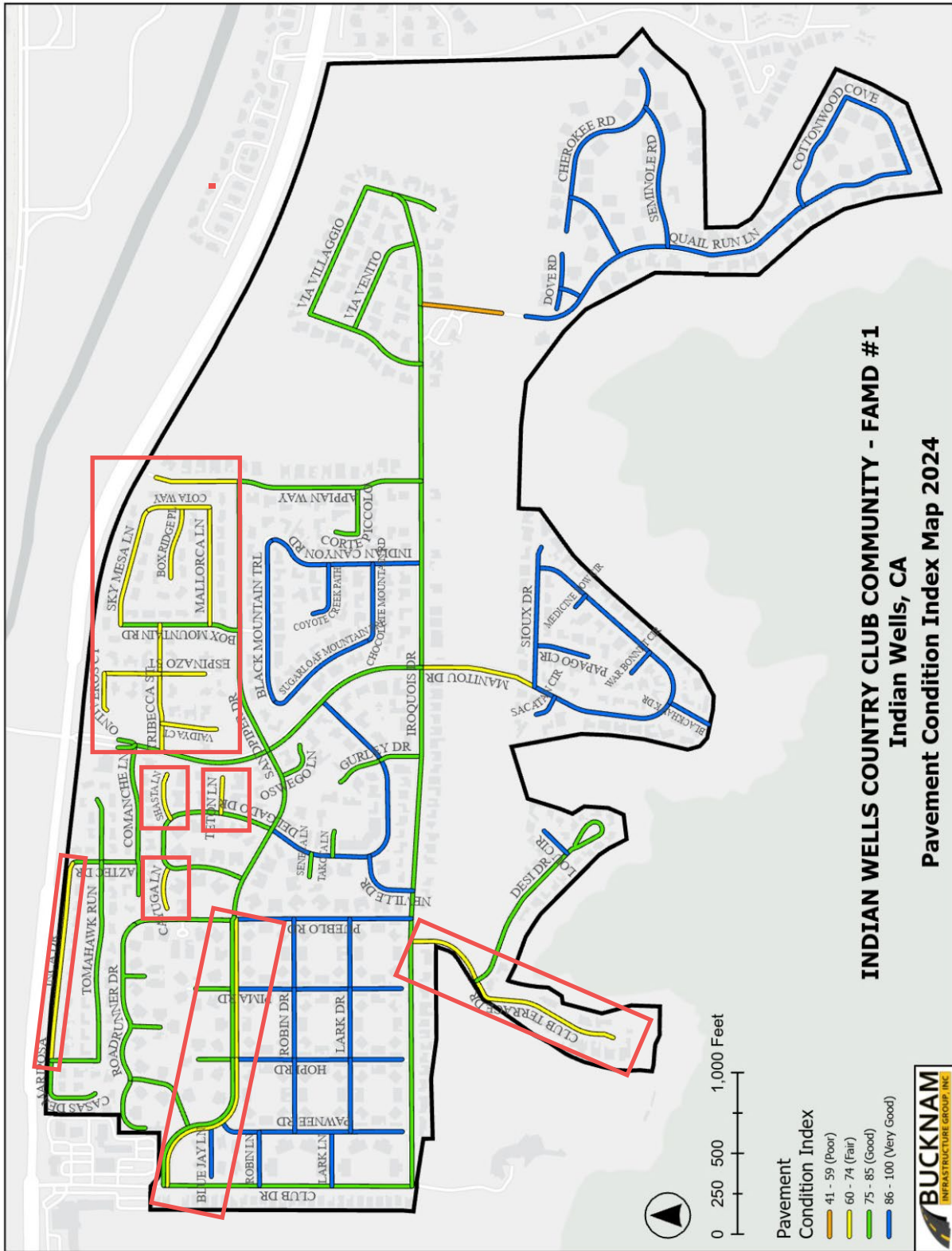


Figure 7 – Fire Access Maintenance District PCI Map - 2024

FINAL REPORT

UPDATE OF
PAVEMENT MANAGEMENT PROGRAM
(Community-wide)

2024-2029



Submitted to:
Indian Wells County Club
Fire Access Maintenance District #1 (Indian Wells, CA)

August 19, 2024



August 19, 2024

Mr. Scott Matas, CCAM
District Manager
Fire Access Maintenance District
42635 Melanie Pl, Suite 103
Palm Desert, CA 92211

Subject: Final Report - Update of the Pavement Management Program

Dear Scott:

As part of the 2024 implementation of the Pavement Management Program for the Fire Access Maintenance District (FAMD) #1, *Bucknam Infrastructure Group, Inc. (Bucknam)* is pleased to submit the Final Report for the Community's pavement network. The information contained in this report was used to develop the recommended improvement program for the pavement network. The report covers the following categories:

- **Executive Summary (Section I)**
- **Pavement Management Program Development and Reporting (Section II)**
- **Pavement Conditions For Each Segment in the Network (PCI Report – Section III)**
The Pavement Condition Index report shows the present condition of all street sections within the pavement network.
- **Forecast Maintenance & Rehabilitation (FMR) Reports (Section IV)**
 - **Recommended Maintenance and Repair Strategies**
The recommended maintenance and repair strategies were used to generate the Forecast Maintenance & Rehabilitation (FMR) Report and were based on our 2024 inspections. Additionally, we have assessed and incorporated unit cost and maintenance application practices/types with our strategies/recommendations.
 - **Projected Projects based on M&R Strategies**
The FMR projects the street maintenance activities recommended for the next five years, broken down to show maintenance levels for Local streets. The report included in Section IV is broken down by fiscal year.



3548 Seagate Way, Suite 230
Oceanside, CA 92056
T: (760) 216-6529
www.bucknam-inc.com

Our thorough analysis of previous and current FAMD PMP strategies enabled our staff to make proactive recommendations for future preventative, rehabilitation and reconstruction projects. All data/comments received from the FAMD have been incorporated in the reports that follow.

It has been a pleasure working with you and the FAMD team on updating your Pavement Management Program. We look forward to the continued success of this project and future teamwork with FAMD staff.

Sincerely,

Bucknam Infrastructure Group, Inc.

A handwritten signature in black ink, appearing to read "Peter J. Bucknam". The signature is fluid and cursive, with a prominent initial "P" and "B".

Peter J. Bucknam
Project Manager
Infrastructure Management – GIS Services

TABLE OF CONTENTS

- I. Executive Summary**
 - A. Fire Access Maintenance District #1 (FAMD) Pavement Network
 - B. Current FAMD Conditions
 - C. Maintenance Strategy Development
 - D. Annual Budget Projections
 - E. Quality Control Efforts
 - F. Findings and Recommendations

- II. Pavement Management Program / Capital Improvement Program**
 - A. Pavement Management Program Scope of Work - 2024
 - B. Maintenance Strategy Assignments
 - C. Multi-Year Annual Work Program Projects
 - i. Maintain PCI Budget Program
 - ii. Recommended Program (Increase PCI to 85)
 - D. Condition Distribution Report
 - E. Calculation of PCI
 - F. Sample Distress Photos – Recommended Treatment

- III. Pavement Condition Index (PCI) Reports**
 - A. PCI Report Definitions
 - B. Fire Access Maintenance District (FAMD) 2024 PCI Map
 - C. Name Order (A-Z)
 - D. PCI Order (0-100)

- IV. Forecast Maintenance & Rehabilitation (FMR) Report**
 - A. Maintain PCI Budget, Maintenance & Rehabilitation Report, Five Year Plan (2024-29)

**Fire Access Maintenance District #1 – Indian Wells
2024 Pavement Management Program
Final Report – August 19, 2024**

<u>Table and Figure Reference</u>	<u>Page #</u>
Figure 1 – PCI Distribution – All Streets (by Section Miles)	Sec 1-4
Figure 2 – Sample Pavement Life Cycle	Sec 2-3
Figure 3 – Resulting Network PCI (Maintain PCI / 5Yr Budget)	Sec 2-7
Figure 4 – Resulting Network PCI (Reach PCI 85 Budget)	Sec 2-9
Figure 5 – PCI Calculation Worksheet	Sec 2-12
Figure 6 – Sample Distress Photos – Recommended Treatment	Sec 2-13
Figure 7 – Fire Access Maintenance District PCI Map – 2024	Sec 3-4

Table 1 – Fire Access Maintenance District #1 PCI Data	Sec 1-3
Table 2 – Condition Distribution by Section Mileage for All Streets	Sec 1-3
Table 3 – Community-wide “Maintain PCI” Budget Projection	Sec 1-7
Table 4 – Five-Year Projection Demonstrating Necessary Budget to Reach PCI of 85	Sec 1-7
Table 5 – PCI Range	Sec 2-2
Table 6 – Maintenance Strategy Assignments	Sec 2-2
Table 7 – Community “Maintain PCI” Budget Projection	Sec 2-6
Table 8 – Five-Year Projection Demonstrating Necessary Budget to Reach PCI of 85	Sec 2-8

Acronym Listing

Asphalt Concrete (AC)
Asphalt Rubber Hot Mix (ARHM)
Average Daily Traffic (ADT)
Capital Improvement Program (CIP)
Fire Access Maintenance District #1 (FAMD)
Geographic Information System (GIS)
Maintenance and Repair (M&R)
Portland Cement Concrete (PCC)
Pavement Condition Index (PCI)
Pavement Management Program (PMP)

SECTION I EXECUTIVE SUMMARY

2024 UPDATE OF PAVEMENT MANAGEMENT SYSTEM

This report reflects the continued dedication and proactive management of the Fire Access Maintenance District #1 (FAMD) Pavement Management Program (PMP); this update of the FAMD PMP is an example of the on-going, strategic planning that will allow FAMD streets to continue to be well-maintained. As the infrastructure network continues to age, street/right-of-way assets, utility assets and operations & maintenance needs will require continuous attention and proactive planning in order to sustain and/or improve the community.

Bucknam Infrastructure Group was contracted to update the FAMD 2024 MicroPAVER dataset which included all defined street locations. From there, Bucknam performed street surface evaluations to generate section-by-section condition values to assist FAMD staff in developing/identifying a multi-year management plan for preventative and capital asphalt projects. The 2024 PMP is essential in that it assists the FAMD Board and staff in identifying necessary annual funding for its street system as well as cost-effectively manages the network through multi-yr inspections, priority maintenance and scheduling.

FAMD staff also shared historical and ongoing maintenance methodologies that have been performed throughout the community over the past 5+ years. In doing this, we were tasked to generate an updated Capital Improvement Program report for the network street locations that recommended potential preventative / rehabilitation projects.

Specifically, this report and program provides administrators, maintenance personnel as well as future PS&E design and asphalt contractor projects/efforts with:

- *The present condition status of the PMP network, as a whole and of any grouping or individual component within FAMD;*
- *A ranked list of streets, or segments of streets, by condition within the network;*
- *Rehabilitation/maintenance needs of each street segment by year;*
- *An optimized priority maintenance and rehabilitation program based on cost/benefit analysis and various levels of funding;*
- *Optimum annual budget levels for pavement maintenance for the next five (5) years; and*
- *Prediction of the future performance of FAMD’s pavement network and each individual street section;*

Pavement is a dynamic structure where deterioration is constantly occurring; thus the pavement management system needs to be updated on a regular basis to reflect these changes in pavement conditions, pavement maintenance histories, and maintenance strategies based upon budgetary constraints. In our approach to develop FAMD’s forecasted maintenance recommendations we worked with staff in identifying unit costs for all maintenance practices used on an annual basis. Currently, based upon FAMD’s maintenance practices and their associated unit costs, the total replacement value of the

defined network is \$28,610,750. This value clearly indicates that FAMD’s pavement network is the most valuable and essential asset. FAMD’s use of slurry seal, conventional / Grind Overlay and R&R practices are typically applied at a five year, ten year and 25 year frequency respectively. These frequencies are typical but FAMD may see increases in deterioration rates due to environmental, load and high average daily traffic (ADT) volumes. For example, sub-surface water intrusion along one of FAMD’s streets will increase deterioration rates within a previously applied AC Overlay compared to portions of the same street section or surrounding street sections. These deterioration rates are monitored through frequent inspections results and deterioration analysis within FAMD’s PMP database.

This report reflects our findings and recommendations for the PMP and the current state of FAMD PMP network. Furthermore, we have recommended detailed funding and maintenance recommendations on the residential network for next five (5) years.

A. FIRE ACCESS MAINTENANCE DISTRICT #1 (FAMD) PAVEMENT NETWORK

Within the defined PMP network there are approximately 12.7 section miles of streets, 102 pavement sections and 2,288,860 SF of Asphalt Concrete (AC) pavement.

FAMD’s pavement network is broken down into manageable groups that have similar characteristics, such as pavement rank, surface type and logical segmentation. Pavement segments are identified by their branch and section numbers. Pavement “branches” that have a common usage, such as Sandpiper Dr, defines a “branch” within MicroPAVER. Pavement “sections” are pavement segments within the defined branch that have consistent pavement rankings, construction/maintenance histories and use. Representative inspection samples are then selected and visually surveyed to locate distress data. This data is used to calculate the pavement sections Pavement Condition Index (PCI) which includes distress type, extent of the distress and its severity.

The PCI is a condition rating that ranges from 100 (a new pavement section or recently overlaid or reconstructed) to 0 for a section that has structurally failed and deteriorated dramatically. Weighted average PCI of a given area = pavement section PCI * its own area divided by the total square footage of the given area. Table 1 summarizes the section conditions found within the pavement network by rank.

The weighted PCI value associated with the Local routes shown through our survey analysis is timely in that it is showing that a large amount of preventative, slurry/cape seal, and overlay work will be needed over the next five year cycle to sustain the level of condition (PCI) within a “preventative maintenance” state.

B. CURRENT FAMD CONDITIONS

The overall condition of FAMD pavement network is “Good” with a weighted average PCI of 83.9 based on the surface area of each segment. The distribution of overall pavement network is shown in Section III of this report (Condition Distribution). For comparison, Bucknam has performed pavement management studies for several other Riverside County local agencies and has included their weighted PCI values; Rancho La Quinta (76.1), Redlands (82.0), Murrieta (79.6), La Cresta HOA (82.3) and Lake Elsinore (78.8).

Mileage	SF	Weighted PCI 2024
12.7	2,288,860	83.9

Table 1 – Fire Access Maintenance District #1 PCI Data

Condition	PCI Range	Section Miles	% of Network
Very Good	86-100	4.8	38%
Good	75-85	5.7	45%
Fair	60-74	2.1	17%
Poor	41-59	0.1	1%
Very Poor	0-40	0.0	0%
		12.7	

Table 2 – Condition Distribution by Section Mileage for All Streets

As shown above, a large majority of segments are evenly distributed through Good to Very Good condition categories. For a network in “preventive” condition status you would typically see Good to Very Good section percentage totals at the 65% to 70% range; FAMD’s network currently shows 83% of its sections within these PCI ranges. These findings indicate that the proper patching, slurry and grind-overlay applications have been performed over the past five years; and furthermore, timely slurry seal / AC grind overlay projects will be needed in the next five years to proactively sustain the overall PCI on the network. These results are now allowing FAMD managers/staff to proactively establish preventative and rehabilitation schedules that will generate further high-value ROI for the Community.

As shown in Table 2, 18% of the FAMD’s network falls within the Fair to Poor condition categories based on PCI, highlighting the need for continued funding and application of proactive slurry seal & overlay projects. More timely overlay rehabilitation activity will increase FAMD’s overall weighted PCI while reducing long-term deferred maintenance costs for fiscal years 2025 thru 2029.

Overlay projects applied to appropriate, qualifying segments will be necessary to achieve a network-wide preventative condition status as described above. A network-wide preventative condition status is typically a network with a weighted average PCI over 78.

Regarding the FAMD PMP network, detailed inspection analysis combined with recent work history and maintenance recommendations indicates that 31% (4.0 miles) of the local pavement network qualifies for slurry seal maintenance activity while 6% (0.8 miles) requires overlay rehabilitation.

Through our assessment of FAMD’s annual pavement maintenance/rehabilitation needs (FY 2024 thru 2029) we have determined that the street network will perform well with continued maintenance, inspections and financial review.

Furthermore, as potential micro-milling, slurry seal and overlay-rehabilitation projects are considered for funding, FAMD should also consider using average daily traffic (ADT) values, sub-grade R - Values, structural design, distress severities and extents as parameters for determining whether a pavement section that lies within the Fair to Very Poor condition range should be overlaid or reconstructed.

We strongly recommend the FAMD staff schedule and perform field meetings with engineering design firms and asphalt concrete contractors to further assess the validity of potential slurry seal/overlay effectiveness, costs and the return on the investment. This planning study will allow staff to initiate those meetings and eventual contracts.

Our PCI conditions reflect “surface” conditions; additional sub-surface data such as coring data, R-Values and ground penetrating radar (GPR) will provide FAMD with a better approach to the maintenance that should be applied.

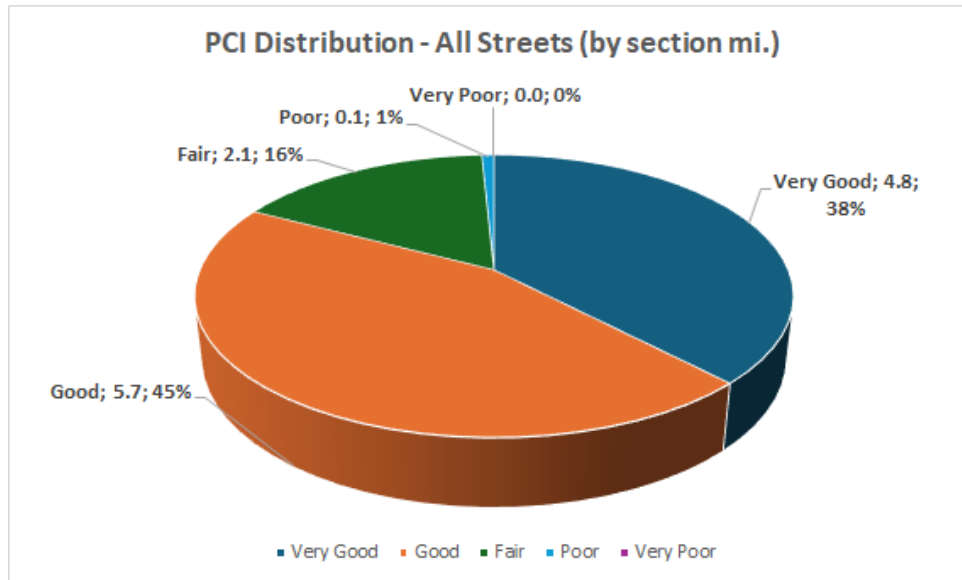


Figure 1 – PCI Distribution – All Streets (by Section Miles)

C. MAINTENANCE STRATEGY DEVELOPMENT

Based on the results of the condition survey and input from the FAMD, pavement maintenance/rehabilitation strategies were verified / developed. At the outset, FAMD and Bucknam staff reviewed the potential distribution of available maintenance funds that would be applied to the PMP network over the next five years. This was based upon the desire to prevent the decrease in street conditions and not allow an increase in the maintenance/rehabilitation backlog over the five-year program.

With this approach, Bucknam has recommended a “minimal level of service” which creates a major dividing line in determining pavement maintenance. Generally within pavement management programs, a PCI range between 55 to 70 determines the threshold of when preventative or major overlay maintenance is activated. Based on FAMD’s weighted average PCI, condition distribution, maintenance practices, our team has identified a PCI of “65” as the minimum level of service. This means that any pavement section with a PCI greater than 65 will typically be recommended for preventative maintenance. This recommendation is indicated in Table 6, Section II.

Bucknam developed a multi-year Capital Improvement Program based on the pavement records, yearly capital expenditures and the most recent 2024 inspections. These recommendations and results are

shown in Section II of this report where we have demonstrated what level of funding is necessary to improve the current weighted condition level of 83 to a level of 85 by FY 2029.

As shown in Figure 1, 17% of FAMD’s streets are in Fair to Poor condition. These sections will be targeted for rehabilitation within our Capital Improvement Program (CIP) recommendations. The reasoning in doing this is to extend the life cycles of those “fair” pavement sections which accrues capital saving to proactively overlay/rehabilitate those pavement sections throughout the rest of the Community that are below the “minimal level of service”.

In order to achieve the most effective and optimum program, certain strategies have been selected and/or analyzed. Below is a listing of the maintenance/rehabilitation activities utilized in strategy development. Each activity is representative of the types of work that have been programmed or recommended as part of the long-term maintenance requirements of FAMD’s street network.

General Repairs-Stop Gap (Localized Maintenance*); PCI range – 20 to 95

For this maintenance type, small localized surface treatments are utilized as “holding action” solutions (stop gaps) to delay the need for pavement structural strengthening. They typically include activities such as crack sealing, AC deep patching, AC skin patching, PCC slab replacement, grinding and leveling.

Micromilling - (Global Maintenance*); PCI range – All

Micromilling is the removal of a minor / small thickness (1” or less) of existing AC prior placing a preventative surface application (i.e., slurry seal). The main difference between conventional cold-milling and micro-milling is the texture left on the remaining surface (*source - LA County of Public Works*).

Microsurfacing - (Global Maintenance*); PCI range – 60 to 85

Microsurfacing is similar to slurry seal. It consists of the application of a mixture of water, asphalt emulsion, aggregate (very small crushed rock), and chemical additives to an existing asphalt concrete pavement surface. Polymer is commonly added to the asphalt emulsion to provide better mixture properties. The major difference between slurry seal and microsurfacing is in how they “break” or harden. Slurry relies on evaporation of the water in the asphalt emulsion. The asphalt emulsion used in microsurfacing contains chemical additives which allow it to break without relying on the sun or heat for evaporation to occur. Thus, microsurfacing is an application that hardens quicker than slurry seals and can be used when conditions would not allow slurry seal to be successfully placed. Streets that have a lot of shade and streets that have a lot of traffic are good candidates for microsurfacing (*source - LA County of Public Works*).

Slurry Seals (Global Maintenance*); PCI range – 60 to 85

Surface treatments applied to pavements with minimal surface distress to provide new wearing surfaces and extend pavement life. Generally consists of a mixture of conventional or latex-modified emulsified asphalt, well-graded fine aggregate, mineral filler and water placed over an existing AC surface; Slurry seal application life-cycles are averaging 4 to 5 years. Type I or II Slurry is recommended for Local streets.

Cape Seals (Global Maintenance*); PCI range – 40 to 65

This is an application of a single layer of asphalt binder to a road surface immediately followed by a single layer of cover aggregate (chips). The single layer chip seal is then followed with a slurry seal application; Conventional cape seal application life-cycles are averaging 6 to 7 years. For sections that have lower PCI's in this range, leveling courses should be considered.

Overlays (Major Maintenance*); PCI range – 20 to 65

AC Overlay – Placement of a layer of hot-mixed asphalt concrete over the existing pavement surface (may include pavement fabric). Grinding (milling) is performed prior to the overlay to reduce the total height of asphalt and assure alignment with existing gutter lines. This also includes “dig-outs” and crack sealing prior to the application of an overlay. This treatment provides a new wearing surface and increased structural strength to the pavement section. A conventional overlay should be designed for a ten-year life.

Asphalt Rubber Hot-Mix Overlay - The ASTM definition is: Asphalt-Rubber is a blend of asphalt cement, reclaimed tire rubber and certain additives in which the rubber component is at least 15% by weight of the total blend and has reacted in the hot asphalt cement sufficiently to cause swelling of the rubber particles. Specifically, using crumb rubber modified binders in pavement application benefit local agencies in that cities find:

- Pavement resists cracking by being more flexible;
- Cost savings come from a longer life cycle (from Bucknam’s experience typically 20% longer), decreased maintenance and the use of less material
- Improvement in skid resistance;
- Decreased noise; and
- It provides long-lasting color contrast for marking and striping
- Life cycles are averaging 8 to 12 years

Reconstruction (Major Maintenance*); PCI range – 0 to 20

Removal of the existing pavement section to a prescribed depth followed by the placement of a conventional flexible pavement section using a structural AC Hot Mix or AR Hot Mix or a full depth asphalt. Each classification of road has a typical design cross-section upon anticipation traffic loading. By performing a reconstruction the sections PCI resets at 100 and restarts the life-cycle deterioration of the section.

*Localized, Global and Major maintenance activities are default terms used within the MicroPAVER pavement software. Specific pavement repair applications are placed within each maintenance activity in order to develop multi-year maintenance forecast recommendations.

D. ANNUAL BUDGET PROJECTIONS

The budgeting process was approached with the following in mind; generate two unique work programs for the next five (5) years based upon actual road pavement conditions in order to:

1. Demonstrate what level of annual funding is necessary to maintain today’s PCI through FY 2029;
2. Identify the required Community-wide budget to reach a PCI level of 85 within five years

Based on current and future pavement maintenance needs, two annual work programs have been prepared and summarized below. Table 3 demonstrates what level of annual funding is needed to “maintain” the conditions found through FY 2029. Table 4 demonstrates the required budget that is needed to improve the community-wide weighted average PCI to a level of 85 within five years.

Plan Year	PCI	Slurry/Cape	Overlay / Recon	Total \$	Deferred Maint.
Current	83.9				\$987,700
2024-25	83.1	\$0	\$0	\$0	\$1,135,400
2025-26	83.1	\$74,400	\$137,800	\$212,200	\$993,600
2026-27	83.0	\$60,900	\$131,300	\$192,200	\$794,400
2027-28	83.2	\$71,600	\$245,200	\$316,800	\$591,800
2028-29	83.0	\$144,800	\$369,800	\$514,600	\$499,200
		\$351,700	\$884,100	\$1,235,800	

Table 3 – Community-wide “Maintain PCI” Budget Projection

Plan Year	PCI	Slurry/Cape	Overlay / Recon	Total \$	Deferred Maint.
Current	83.9				\$987,700
2024-25	83.1	\$0	\$0	\$0	\$1,135,400
2025-26	83.4	\$93,100	\$137,800	\$230,900	\$909,000
2026-27	83.9	\$80,500	\$170,600	\$251,100	\$635,300
2027-28	84.6	\$92,400	\$311,200	\$403,600	\$331,700
2028-29	85.1	\$205,700	\$409,800	\$615,500	\$162,700
		\$471,700	\$1,029,400	\$1,501,100	

Table 4 – Five-Year Projection Demonstrating Necessary Budget to Reach PCI of 85

Note: Within both budget scenarios above, the District does have slurry or overlay projects scheduled for FY 2024-25

Additional detail and breakdown of budget projections are demonstrated in Section IV of this report. All work program budgets generated are presented in terms of current 2024 dollars. All repair activities were based on distresses observed at the time of the field survey. These are recommendations and are to be used as “the best case scenario” for improving the FAMD street network.

E. QUALITY CONTROL EFFORTS

This project included the implementation and build of the FAMD MicroPAVER 2024 database, survey of all streets; true area adjustments were made to the database for all AC pavement sections. The goal is to assess the 2024 PMP study in order to consider and recommend a Community-wide PMP study during FY 2024-25.

As indicated in our scope of work, Bucknam performed initial quality control checks in the field prior to survey to gain an understanding of the current conditions resulting from previous maintenance and long-term deterioration. During survey efforts additional samples / field check efforts were performed to notate section by section load bearing distresses.

F. FINDINGS AND RECOMMENDATIONS

Pavement management involves frequent preventative maintenance; as pavement deteriorates through heavy traffic impacts, weathering and time, preventative maintenances (such as slurry seal, stop gap, etc.) have limited benefits. Our study has shown that key local overlay projects and Community-wide slurry seal will be needed over the next five years to maintain the network’s high level of condition.

Currently, FAMD’s PMP network holds above average weighted PCI values (80+); it is our recommendation that a proactive, common sense slurry/overlay program be assessed and scheduled over the next several fiscal years. This will ensure that the Community-wide weighted PCI will sustain itself and allow for routine maintenance/rehabilitation to continue. Through our analysis of the PMP we have found and recommend the following items which should be considered for a proactive approach to future management of the program:

LOCAL FINDINGS AND RECOMMENDATIONS

Our PCI calculations have indicated that the FAMD Local network is currently in “Good” condition (weighted avg. PCI of 83.9). At a minimum, to maintain this condition, it is critical that preventive maintenance and overlay activities are funded at the levels identified on Table 3 to maintain a “Good” weighted average PCI value.

Our Local network findings for conditional data and recommendations for revenue expenditures are shown below:

- The Local network has a weighted PCI of 83.9;
- The Local network consists of 12.7 miles and 2,288,860 SF of pavement;
- Currently, 6% of the Local network (126,408 SF) qualify for slurry seal maintenance while 34% (785,848 SF) qualifies for rehabilitation;
- At a minimum, Local maintenance projects should focus on the maintaining the current PCI at an weighted average of 83 over the next five years;
 - Maintain the Local revenues at an average of \$247,200 /yr level for the term of the CIP to generate the projects identified on Table 3;
- Develop a proactive fiscal and planned approach to identify local overlay projects based on the deterioration modeling within MicroPAVER;
 - Demonstrated budget shown in Section I, Table 4 is ample increase the Local weighted PCI of 83 to 85 after five years (approx. \$300,200 annually), additionally, the network deferred backlog decreases from a level of \$988k to \$163k after five years; and
- Perform pavement inspections on the entire FAMD network every two years to continue FAMD’s solid planning model within MicroPAVER to track PCI deterioration.

SECTION II PAVEMENT MANAGEMENT SYSTEM / CAPITAL IMPROVEMENT PROGRAM

Bucknam Infrastructure Group, Inc. (Bucknam) performed the following services in accordance with the scope of services that was contracted with the FAMD. As a quick overview, the following tasks were performed to complete the work over the past several months:

2024 Pavement Management Work Efforts:

- Task 1:** Project Kickoff-Data Management
- Task 2:** Update of Maintenance Activities
- Task 3:** Pavement Condition Survey (approx. 12.7 miles)
- Task 4:** Budgetary Analysis and Capital Improvement Reports
- Task 5:** Executive Summary and Final CIP Reports

A. Pavement Management Program Scope of Work - 2024

As a part of the 2024 update of the pavement management system, a major element of work was to complete a comprehensive assessment of the existing street network and PMP database. This included assessing available digital community maps, street naming conventions and work history information. From there, Bucknam worked with staff to confirm public street listings which set the foundation for accurate surveying and CIP reporting. All data was then published within the FAMD MicroPAVER database (currently being maintained by Bucknam; FAMD did not purchase the software for this project).

Work history information was provided by staff in the form of field notes, institutional knowledge, and Adobe documents. This information was entered into the proper pavement segments that match the limits of those projects. From there, CIP pavement recommendations were performed (discussed and demonstrated below) where the pavement maintenance information staff provided (PMP material practices, unit costs, and capital budgets) were used to generate recommendations through the MicroPAVER system.

Table 5 demonstrates PCI ranges defaulted within MicroPAVER. Once a pavement inspection is complete, a PCI is calculated for each pavement section. Each PCI calculated falls within a defined PCI range category (Very Good, Poor, etc.). Furthermore, a weighted PCI was calculated for the each functional class within the network (locals).

The PCI is a condition rating that ranges from 100 (a new pavement section or recently overlaid or reconstructed) to 0 for a section that has structurally failed and deteriorated dramatically. Weighted average PCI of a given area = pavement section PCI multiplied by its own area divided by the total square footage of the given area. This information can also be represented through MicroPAVER to show how much square footage or percentage of area falls within a PCI range category.

<u>PCI RANGE</u>	<u>CONDITION</u>
86-100	Very Good
75-85	Good (FAMD PMP Network 2024 = 83.9)
60-74	Fair
41-59	Poor
0-40	Very Poor

Table 5 - PCI Range

These condition ranges are defined by the Army Corps of Engineers and defaulted within the MicroPAVER software. The summary of all roads condition data and their representative PCI's can be seen in the Pavement Condition Report in Section III.

B. MAINTENANCE STRATEGY ASSIGNMENTS

Once the appropriate activities from the above listings were shared by FAMD, a Maintenance Strategy Table was defined within the system that allocated the appropriate actions to the specific repair needs of the street. In defining the maintenance strategy list, emphasis was placed on defining pavement condition thresholds and using the PCI for the specific maintenance activities within these categories.

Maintenance Strategy Assignment Table

All Streets		
PCI Range	Description	Unit Cost
20-100	Preventative, Stop Gap, Patching	Varies by Activity
Varies by Activity		
60-85	Type II Slurry	\$0.40/SF
Minimal Level of Service (65)		
60-85	Cape Seal	\$1.60/SF
20-60	AC Grind-Overlay	\$3.80/SF
20-60	ARHM Grind-Overlay	\$4.25/SF
0-20	AC Reconstruction	\$12.50/SF
0-20	PCC Reconstruction	\$22.50/SF
<i>10% Contingency included within All Unit Costs</i>		

Table 6- Maintenance Strategy Assignments

The Strategy Assignments List, shown in Table 6, was developed to identify the most critical segments in each of the work programs.

Segment priorities were established by determining the range of PCI's requiring first attention based on the relative value of each segment's PCI, thus maximizing the annual maintenance budget. Also, distress quantity, area extent, type and severity were critical elements in the decision process for recommending maintenance. The assignment table is used as a guide within MicroPAVER to recommend maintenance,

however, further assessment by staff and/or outside parties can override maintenance recommendations. This can be done by reviewing and assessing distress extents and their weighted percentages.

Once the strategy assignments were set within the system, budgets and work assignments were generated for each work program on an annual basis. Using pavement deterioration curves for each type of pavement surface and class of road, both current year and future years work requirements for each pavement segment were determined. In forecasting the maintenance requirements in future years, the current PCI value is reduced annually for each pavement segment based on the MicroPAVER deterioration curves within the FAMD database.

Likewise, maintenance activities performed in a given year increase the PCI value as they are applied to the segment. The overall program is dynamic in that each strategy consists of a cyclic series of actions that simulates the pavement anticipated life cycle.

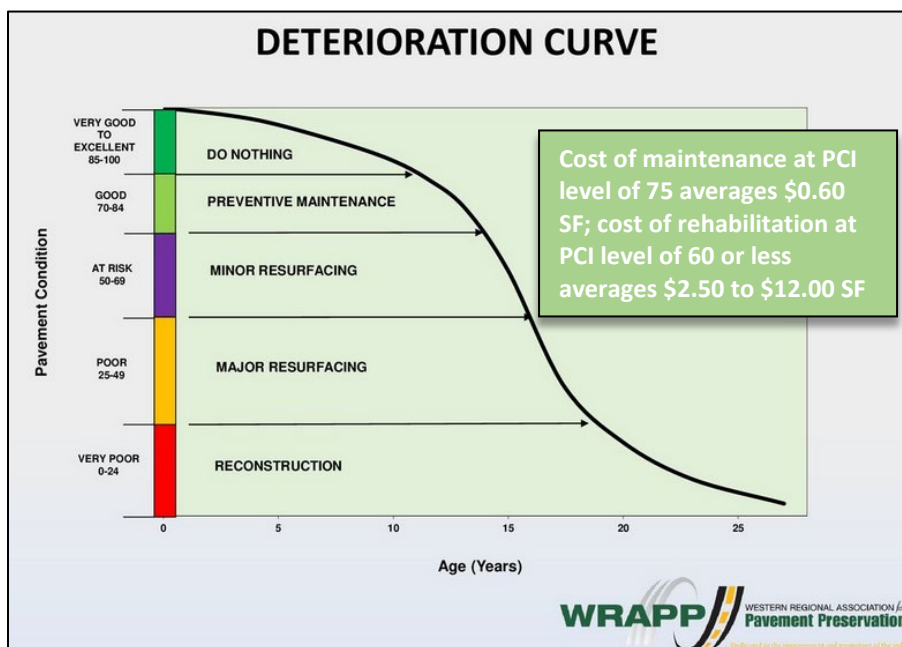


Figure 2 – Sample Pavement Life Cycle

(image source – WRAPP Western Regional Association for Pavement Preservation)

C. MULTI-YEAR ANNUAL WORK PROGRAM PROJECTIONS

The goal of these projections is to assist FAMD policy makers in utilizing the recommendations of the MicroPAVER system. By using FAMD’s current projected budgets and maintenance practices the system will develop “section unique” improvements and strategies. Each segment will be tied to a specific fiscal year. As shown in the following pages, we have assessed the budgets that have been projected to meet the maintenance and rehabilitations needed to maximize your return on investment. The budget forecasting goal for the FAMD network focused on:

- ❖ Establishing a proactive multi-year Maintenance & Rehabilitation Program;
- ❖ Developing a preventive maintenance program; and
- ❖ Selecting the most cost-effective repairs based on FAMD strategies

MAINTAIN BUDGET – The Maintain budget (\$247,200/yr for five years) was generated to demonstrate what level of funding is necessary to sustain/maintain today’s conditions through FY 2029;

INCREASE PCI BUDGET – This recommended budget was generated to demonstrate the necessary funding required to increase the current weighted PCI level of 83 to 85 after five years.

LOCAL BUDGET PROJECTIONS

The annual projected revenues shown below only account for the cost of pavement maintenance and rehabilitation activities (edge of pavement to edge of pavement).

1. 10% contingency costs were applied to pavement material costs; additional soft costs that were not included were:
 - a. Right-of-way improvements
 - b. Curb & gutter improvements
 - c. ADA ramp improvement
 - d. Utility improvement
 - e. Design, construction management, inspection, testing
 - f. Tree removals
2. All multi-year budget projections include a 4% unit cost inflation rate for the term of the budget forecast

MAINTAIN – A common question from constituents and board members is “what level of funding is necessary to maintain the asset?” Bucknam modeled this scenario against the conditions found through our recent field surveys. Several iterations were modeled and assessed for viability and a common sense approach to FAMD’s PMP network. The resulting PCI conditions and maintenance distributions are shown below.

MAINTAIN PCI BUDGET PROGRAM

This budget program incorporates pavement sections that have a functional class of Locals (E).

Plan Year	PCI	Slurry/Cape	Overlay / Recon	Total \$	Deferred Maint.
Current	83.9				\$987,700
2024-25	83.1	\$0	\$0	\$0	\$1,135,400
2025-26	83.1	\$74,400	\$137,800	\$212,200	\$993,600
2026-27	83.0	\$60,900	\$131,300	\$192,200	\$794,400
2027-28	83.2	\$71,600	\$245,200	\$316,800	\$591,800
2028-29	83.0	\$144,800	\$369,800	\$514,600	\$499,200
		\$351,700	\$884,100	\$1,235,800	

Table 7 – Community-wide “Maintain PCI” Budget Projection

By modeling the existing pavement conditions utilizing benchmarked FY 2024 Southern California local agency unit costs, we have found that two positive results occur over the five year CIP. (See Figure 3 on the following page). First, the weighted PCI for the entire network does maintain itself (83.9 to 83.0) over the five year period based upon an average \$247,200/yr budget. This result is positive in that the PCI does not drop for a long-term, furthermore, as described below, the amount of deferred maintenance that remains on the network by FY 2029 is cut in half and is a goal that the Community should strive for.

The resulting deferred maintenance backlog shows that it decreases to a lower level (\$988k to \$499k) after the five years program which indicates that an annual \$247,200 budget is ample to proactively reduce the deferred maintenance on the network by 2029. If limited annual funding for the qualifying overlay projects is applied; this in turn will defer maintenance/rehabilitation to latter years of the CIP thus increasing the costs of maintenance. This problem will continue to build upon itself unless an influx of overlay revenue is generated by the Community.

DEFERRED MAINTENANCE
 Delaying repairs on streets where pavement condition indicates a need creates deferred maintenance. Deferred maintenance includes pavement maintenance / rehabilitation that is needed across the entire network, but cannot be performed due to the lack of available funding and is pushed to the next budget cycle. The actual repairs that are being deferred are often referred to as a “backlog”. As maintenance is deferred, the opportunity to apply life extending preventive pavement applications is lost and the ultimate cost of rehabilitation multiples.

As shown, this projection model does meet the initial goal of maintaining FAMD’s pavement network PCI. In order for these scenarios to continue to produce these results proactive and continued funding is

necessary; on an annual basis, FAMD should monitor the management of overlay deferred rehabilitation. The potential delay in projects and the resulting buildup of more overlay work in the five-year time frame is not a debt that FAMD will want to accrue.

The Local maintenance model that has been developed under the Maintain budget can be used as a benchmark to monitor annual budget allocations as the network continues to mature and age; the proper amount of funding for slurry seal and overlay maintenance needs to be FAMD highest priority. Additionally, it is recommended that staff continue to monitor the application of Cape Seal or Asphalt Rubber Hot Mix (ARHM) as an asphalt application for the specific Local sections. Specific sections are now qualifying for rehabilitation that warrants a stronger application rather than a typical slurry seal. With a five year cycle in motion, it is essential to address Local sections that have PCI's less than 65 with the proper maintenance/rehabilitation.

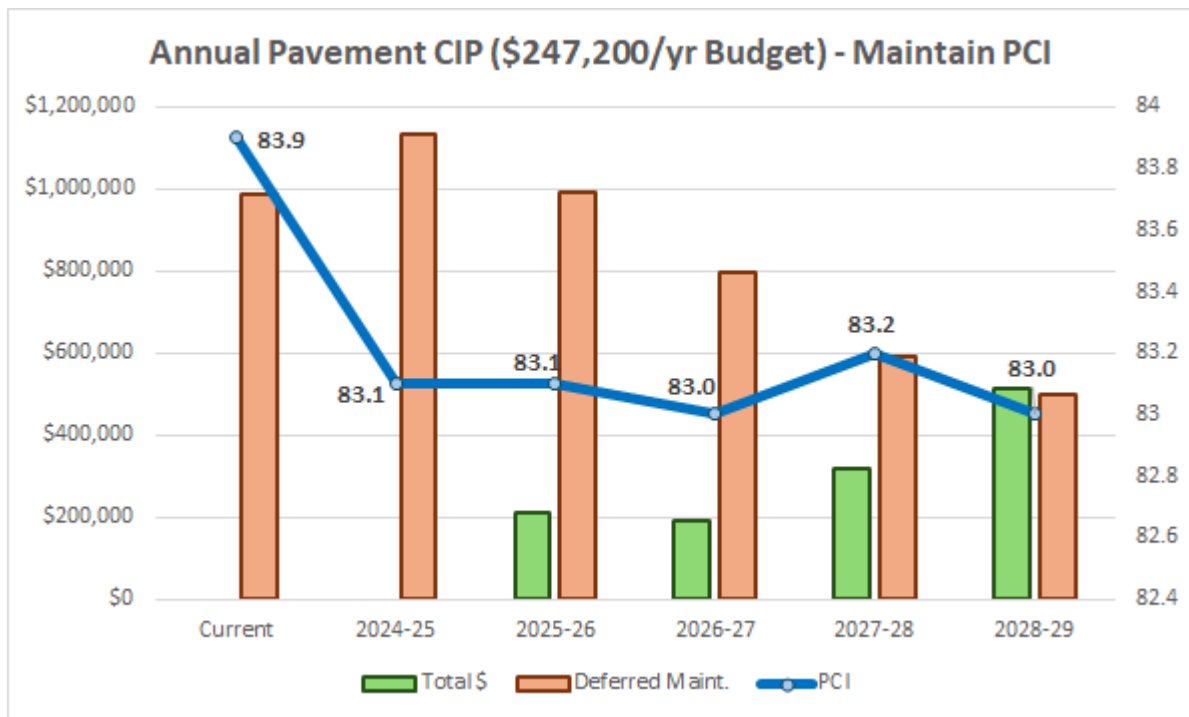


Figure 3 – Resulting Network PCI (Maintain PCI / 5yr Budget)

The resulting “maintaining of the weighted PCI” shown above demonstrates how applying adequate capital funds to specific sections of the network allows the pavement to sustain itself at a rate that is conducive to a successful PMP (i.e. proper/timely application of preventive maintenance and extension of section life-cycles through timely overlays). Additionally, even with these budget results, staff should continue to apply localized maintenance (i.e. deep patching, leveling courses, crack sealing, etc.) prior to any major slurry seal maintenance and/or overlay rehabilitation. By performing stop gap measures to individual pavement sections the overall performance of the sections condition will improve over time and sustain itself longer than if no preventive maintenance was performed.

On the flip side, if insufficient annual funding is applied to the network over the next five years (i.e. budget of \$50,000/yr) a decrease in overall weighted PCI will occur and deferred maintenance/CIP projects will

increase to a level of \$1,525,000 by FY 2029. Adequate funding equals timely projects which allows necessary overlay projects to be completed in a timely manner on the local network (see recommended budget below).

RECOMMENDED PROGRAM (INCREASE PCI TO 85)

With FAMD striving to show proactive maintenance across all streets, a recommended budget program was generated to show the greatest return on investment through the application of slurry seal, grind/overlay and alternative overlay rehabilitations. Our goal is to increase the network PCI level from 83 to 85 after five years.

With this scenario, our initial goal is to provide staff with a budgetary outlook and conditional impact report that can be used to create a solid capital overlay and preventative slurry seal maintenance program.

Again, we used the Maintain PCI model (shown above) as a cornerstone for our modeling within this recommended program. Combining the previous reporting models with a realistic and achievable annual budget, we found positive results.

The Reach PCI 85 Budget Program incorporates pavement sections that have a functional class of Locals (E).

Plan Year	PCI	Slurry/Cape	Overlay / Recon	Total \$	Deferred Maint.
Current	83.9				\$987,700
2024-25	83.1	\$0	\$0	\$0	\$1,135,400
2025-26	83.4	\$93,100	\$137,800	\$230,900	\$909,000
2026-27	83.9	\$80,500	\$170,600	\$251,100	\$635,300
2027-28	84.6	\$92,400	\$311,200	\$403,600	\$331,700
2028-29	85.1	\$205,700	\$409,800	\$615,500	\$162,700
		\$471,700	\$1,029,400	\$1,501,100	

Table 8 – Five-Year Projection Demonstrating Necessary Budget to Reach PCI of 85

Referring to Table 8, it is noted that the weighted PCI increases at a consistent pace to 85 throughout the five-year projection. Furthermore, the annual deferred maintenance decreases from \$988k to \$163k after the five-year term if FAMD utilizes an annual average of \$300,200/yr for slurry maintenance and overlay rehabilitation. We found annual average square footage breakdowns were consistent and well balanced; combined with the positive results found with the weighted PCI and deferred maintenance we recommended that this 5-yr maintenance schedule be followed.

In summary, the FAMD network is performing well and above average. Consistent and frequent audits of the network should continue through Board management, constituent input, biennial inspections and financial reporting. This will ensure that all parties are clearly seeing how the network is performing based upon its long-term management.

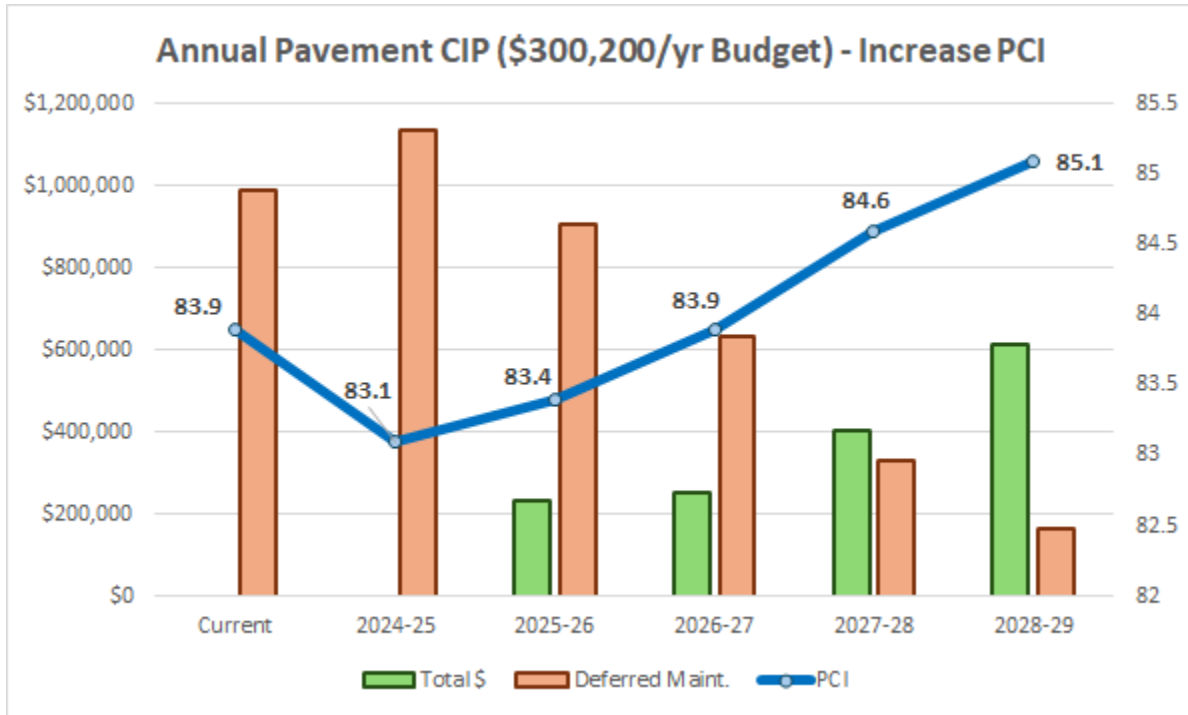


Figure 4 – Resulting Network PCI (Reach PCI 85 Budget)

PAVEMENT MANAGEMENT PROGRAM REPORTS

In addition to the annual budget scenario, this report contains a comprehensive and complementary assemblage of pavement management reports ranging from summary reports to annual maintenance and rehabilitation schedules (Forecast Maintenance & Rehabilitation (FMR) Report, Section IV). Collectively as well as individually, the reports represent reasonable projections of pavement maintenance needs and performance based on visual condition assessments, unit cost estimates, and pavement deterioration models.

It is important to note that pavement segment dimensions and surface area recorded during 2013, 2020 and 2024 inspections, along with the action and repair costs, as presented within the reports are accurate within tolerable limits. This is noteworthy due to the "implied" accuracy of reporting length and width to the nearest foot, surface area to the nearest square foot, and action and repair unit costs and project estimates to the nearest penny and dollar, respectively.

NEXT STEPS

As with any infrastructure management software program, time investments need to be made by key staff to maintain the integrity of the data as well as its accuracy. Bucknam can perform annual work history (patching, slurry, overlay) updates to the FAMD database to ensure accurate PCI deterioration/improvement tracking. This will be key to future management of the pavement program and reporting. FAMD personnel need to maintain their commitment to the preventive maintenance system, while working toward reducing present backlog of rehabilitation projects.

In order to ensure that report outputs are accurate and credible, it is essential that the integrity of all data files be maintained. This will require performing all necessary updates when changes are made to scheduling scenarios, unit cost information, historical data, etc. **In addition, the entire pavement network should be evaluated every two to three years through PMP AC/PCC inspections, reporting and long-term planning analysis.**

This will not only allow work to be scheduled based on the most current condition data available, but will provide personnel with a means to monitor actual rates of pavement deterioration so appropriate modifications can be made to the system curves.

Through potential future as-needed contracts, Bucknam will be provide FAMD with staff level support to assist in the continuous updates with the MicroPAVER system. This will include work history updates, generating reports from the system, unit cost updates, and future inspections.

D. CONDITION DISTRIBUTION REPORT

This report depicts the distribution of the pavement condition throughout the street network by area.

The condition scheme ranges from “Very Good” to “Very Poor”; with a “Very Good” condition corresponding to a pavement at the beginning of its life cycle, and a “Very Poor” condition representing a badly deteriorated pavement with virtually no remaining life.

The table below shows the general description for each pavement condition:

Condition Description – PCI Range - Description

- Very Good (86-100)** - Very Good condition describes a pavement whose surface is intact and without cracking, rutting or potholes. In addition, the ride quality and appearance are excellent. The only maintenance required on the streets is frequent sweeping and slurry seal application at 5 to 7 year intervals

- Good (75-85)** - Good condition is a pavement where approximately 70% of more of the overall surface is intact while the remaining section of the pavement may have minor rutting, moderate pavement distresses, occasional utility or repair cuts and other minor distresses. These streets may be in need of “minor maintenance work” such as crack sealing and patching followed by slurry seal (**FAMD weighted average PCI is 83.9**).

- Fair (60-74)** - Fair condition describes a pavement whose surface area has frequent cracks, surface degradation, minor if any subsurface failure, occasional potholes and reduced ride quality. A new material overlay will provide additional structural strength to overall pavement.

- Poor (41-59)** - Poor condition describes a pavement whose surface exhibits substantial visible cracks, significant rutting, several potholes or utility cuts, reduced ride quality and subsurface failures.

- Very Poor (0-40)** - Very Poor is reserved for totally deteriorated pavements. A pavement in this classification exhibits obvious substantial cracks, frequent potholes, rutting and utility repairs, uncomfortable ride quality and inefficient drainage. The cure this condition is a total removal of existing pavement and base materials followed up with total reconstruction.

E. CALCULATION OF PCI

In order to calculate a Pavement Condition Index (PCI) value within MicroPAVER, specific street section data needs to be inputted into MicroPAVER to define the survey limits, asphalt types, pavement age and metrics. Pavement “sections” are pavement segments within the defined branch that have consistent pavement street classifications, construction/maintenance histories and use. Representative inspection samples are then selected and visually surveyed to locate distress data. This data is used to calculate the pavement sections Pavement Condition Index (PCI) which includes distress type, extent of the distress and its severity.

The PCI is a condition rating that ranges from 100 (pavement section that is in perfect condition) to 0 for a section that has structurally failed and deteriorated dramatically. The PCI is calculated from three major data entries from our inspectors:

1. Distress Type (one of 20 AC or 19 PCC types); these include alligator cracking, bleeding, block cracking, corrugations, depressions, long/trans cracking, patch/utility cut, potholes, rutting, weathering, raveling, etc.
2. Distress Quantity (the square footage, length or count of a specific distress)
3. Distress Severity (the level of severity determined for each distress found; low, medium or high)

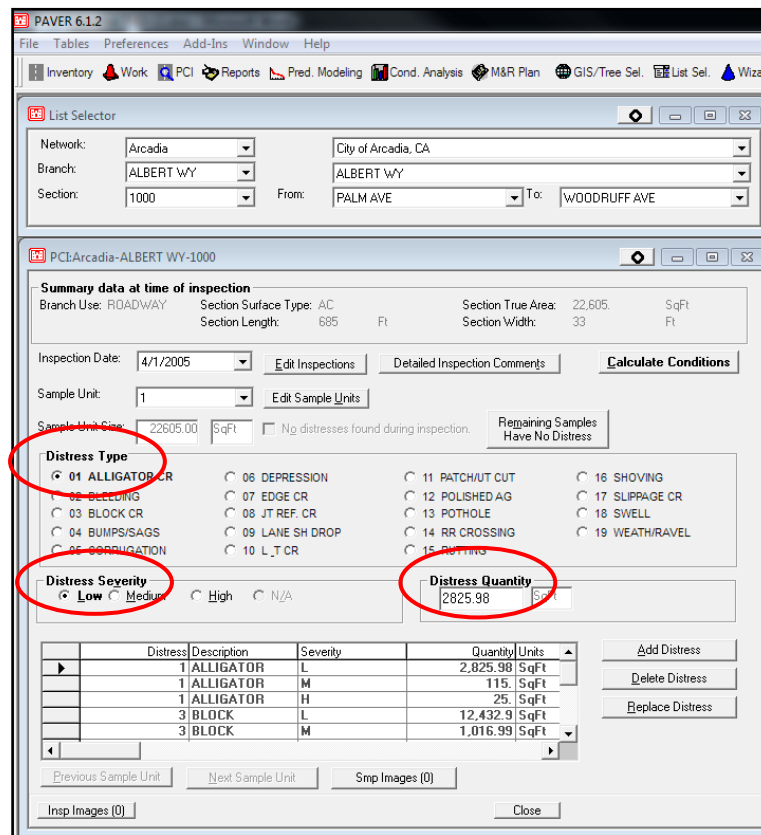


Figure 5 – PCI Calculation Worksheet

F. SAMPLE DISTRESS PHOTOS – RECOMMENDED TREATMENT (FIGURE 6)

Bucknam Infrastructure Group



1. Alligator Cracking



Cracks that form a chicken wire or alligator scale like pattern.

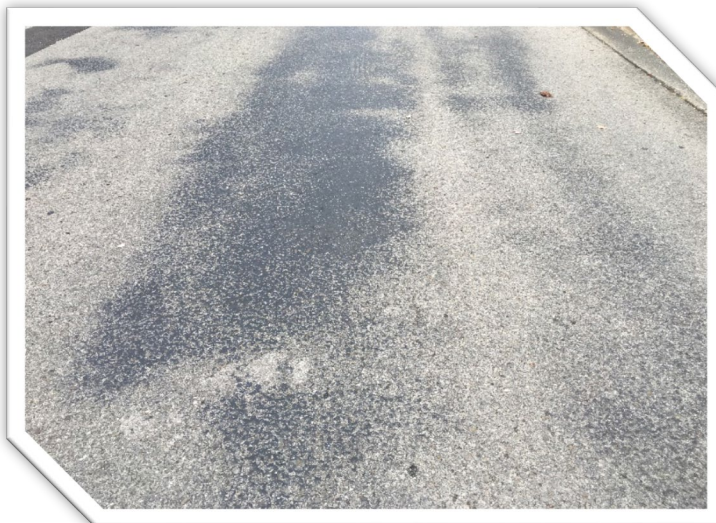
Low Severity: Thin parallel longitudinal cracks that may come together at certain points, but full alligator pattern is not present yet.

Medium Severity: Further development of cracks into alligator pattern. Cracks are starting to spall.

High Severity: Alligator pattern is heavily developed, and cracks are spalled to the point where individual pieces may become separated.

Typical Recommendation: Low severity, R&R – Patching, crack sealing; high severity R&R-overlay

2. Bleeding



Bleeding occurs when incorrectly mixed asphalt is applied and in hot weather the asphalt or tar rises to the surface.

Severity is determined by the amount of asphalt/tar present.

Typical Recommendation: Low severity, apply coarse sand; high severity, grind or heat planer excess, resurfacing may be necessary

3. Block Cracking



Longitudinal and transverse cracks that intersect to form smaller than 10x10 ft blocks. Creates uniform blocks with straight edges.

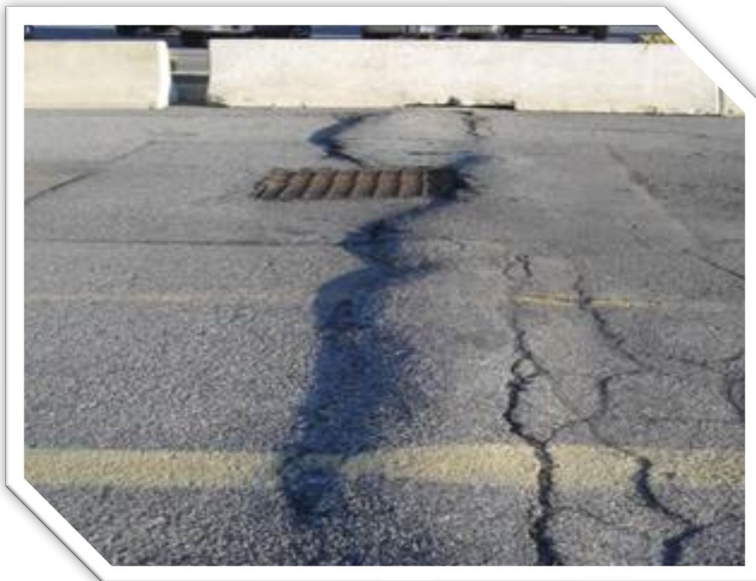
Low Severity: Cracking is less than 3/8 inches.

Medium Severity: Cracking between 3/8 and 3 inches.

High Severity: Cracking is over 3 inches.

Typical Recommendation: Low severity, crack sealing; high severity, R&R-overlay

4. Bumps and Sags



Small, localized, and linear upward or downward displacements of pavement, which can be caused by a variety of factors.

Severity is determined by the extent to which ride quality is diminished.

Typical Recommendation: R&R - Patching

5. Corrugation



Closely spaced Bumps and or Sags that form a washboard effect in the pavement.

Severity is determined by the extent to which ride quality is diminished.

Typical Recommendation: Low severity, R&R – Patching; high severity, R&R-overlay

6. Depression



Localized area of pavement with a lower elevation than the surrounding pavement.

Low Severity: depth of ½ to 1 inch.

Medium Severity: depth of 1 to 2 Inches.

High Severity: depth greater than 2 inches.

Typical Recommendation: R&R - Patching

7. Edge Cracking



Cracks that are parallel to the edge of the pavement that may cause a break up of pavement.

Low Severity: Low or Medium cracking with no breakup.

Medium Severity: Medium cracking with some breakup.

High Severity: Considerable breakup of pavement.

Typical Recommendation: R&R - Patching

8. Joint Reflective Cracking



Cracking that is reflected through AC pavement when it is overlaid on top of PCC pavement.

Low Severity: Cracking is less than 3/8 inches.

Medium Severity: Cracking between 3/8 and 3 inches.

High Severity: Cracking is over 3 inches.

Typical Recommendation: R&R - Overlay

9. Lane / Shoulder Drop-off



Elevation change between pavement and shoulder.

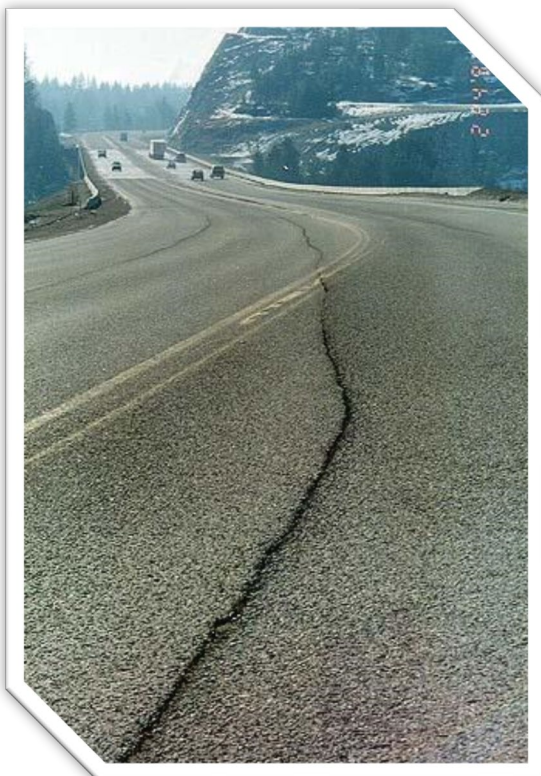
Low Severity: Difference in elevation is between 1 and 2 inches.

Medium Severity: Difference in elevation is between 2 and 4 inches.

High Severity: Difference in elevation is over 4 inches.

Typical Recommendation: R&R – Patching or edge grinding

10. Linear & Transverse Cracking



Cracks that are generally either parallel or perpendicular to traffic.

Low Severity: Cracking is less than 3/8 inches.

Medium Severity: Cracking is between 3/8 and 3 inches.

High Severity: Cracking is over 3 inches.

Typical Recommendation: Low severity, crack sealing; high severity, R&R - Overlay

11. Patching



Area of pavement that has been replaced.

Severity is determined by the quality of the patch and the extent to which ride quality is diminished.

Typical Recommendation: R&R – structural / non-structural overlay

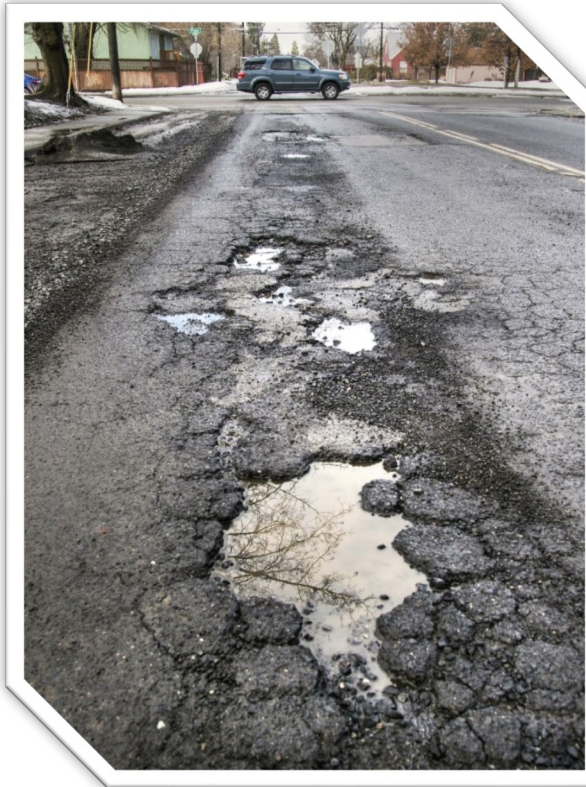
12. Polished Aggregate



Distress where traffic smooths the pavement surface so friction is diminished and cars can slide.

There are no Severity Levels for this distress.

13. Pothole



Severity Measured using the following Matrix.

Maximum Depth Of Pothole (in.) (mm)	Average Diameter (in.) (mm)		
	4 to 8 in. (100 to 200 mm)	8 to 18 in. (200 to 460 mm)	18 to 30 in. (460 to 760 mm)
1/2 to ≤ 1 in. (13 to 25 mm)	L	L	M
> 1 to ≤ 2 in. (25 to 50 mm)	L	M	H
> 2 in. (50 mm)	M	M	H

Typical Recommendation: low severity Pothole fill or R&R – Patching, high severity should be R&R-Overlay

14. RR Crossing



Pavement distresses caused by railroad crossings.

Severity is determined by the extent to which ride quality is diminished.

Typical Recommendation: R&R - Patching

15. Rutting



Linear depressions along wheel paths caused by traffic.

Low Severity: Depth is $\frac{1}{4}$ to $\frac{1}{2}$ inches.

Medium Severity: Depth is $\frac{1}{2}$ to 1 inch.

High Severity: is greater than 1 inch.

Typical Recommendation:
Pavement with deeper ruts should be leveled and overlaid

16. Shoving



Displacement of pavement creating a “wave” over a more solid surface.

Severity is determined by the extent to which ride quality is diminished.

Typical Recommendation: R&R - Patching

17. Slippage Cracking



Half-moon shaped cracks where wheels cause pavement to slide.

Low Severity: Average crack width is less than 3/8 inch.

Medium Severity: Crack width is between 3/8 and 3/2 inches.

High Severity: Crack width is greater than 3/2 inches.

Typical Recommendation: R&R - Patching

18. Swell



Upward Bulges creating “wave-like” patterns.

Severity is determined by the extent to which ride quality is diminished.

Typical Recommendation: Low severity, R&R – Patching; high severity, R&R-overlay

19. Weathering



The wearing away of the asphalt binder.

Low Severity: Aggregate is starting to be exposed.

Medium Severity: Aggregate is exposed up to ¼ of its width.

High Severity: Aggregate is exposed to greater than ¼ of its width.

Typical Recommendation: naturally occurring, slurry seal

20. Raveling



The further weathering of asphalt so that coarse aggregate is separating out of pavement.

Medium Severity: Considerable loss of aggregate.

High Severity: Almost complete removal of coarse aggregate.

Typical Recommendation: Low severity, R&R – Patching; high severity, R&R-overlay

PORTLAND CEMENT CONCRETE (PCC)

1. Blowup



Buckling at cracks or joints where there is not enough room for slab expansion.

Severity is determined by the extent to which ride quality is diminished.

2. Corner Break



Crack close to corner of slab that creates a corner piece.

Low Severity: Crack is less than ½ inches wide.

Medium Severity: Crack is between ½ and 2 inches wide.

High Severity: Crack is wider than 2 inches.

3. Divided Slab



Slab that is broken up into four or more pieces by cracks.

Severity is determined by the following matrix.

Severity Of Majority Of Cracks	Number Of Pieces In Cracked Slab		
	4 to 5	6 to 8	More than 8
L	L	L	M
M	L	M	H
H	M	H	H

4. Durability Cracking



Pattern of cracks parallel to joints caused by freeze-thaw expansion of large aggregate.

Low Severity: Durability cracking covers less than 15 percent of slab.

Medium Severity: Durability cracking covers more than 15 percent of the slab.

High Severity: Durability cracking covers more than 15 percent of slab and most pieces have come out.

5. Faulting



Elevation Difference between slabs.

Low Severity: Elevation difference is between 1/8 and 3/8 inch.

Medium Severity: Elevation is between 3/8 and 3/4 inch.

High Severity: Elevation is greater than 3/4 inch.

6. Joint Seal Damage



Damage to sealant between joints that allows soil, rock, or water infiltration.

Low Severity: Joint sealant has only minor damage.

Medium Severity: Joint sealant is in fair condition. Water can infiltrate and vegetation may be present.

High Severity: Joint sealant is in poor condition. It may be missing and rocks may be present.

7. Lane / Shoulder Drop-Off



The Elevation difference between pavement and shoulder.

Low Severity: Elevation difference is between 1 and 2 inches.

Medium Severity: Elevation difference is between 2 and 4 inches.

High Severity: Elevation difference is greater than 4 inches.

8. Linear Cracking



Cracks that divide slab into two or three pieces.

Low Severity: Crack is less than ½ inch wide.

Medium Severity: Crack is between ½ and 2 inches wide.

High Severity: Crack is wider than 2 inches.

9. Large Patch



Patch that is larger than 5.5 sq ft.

Low Severity: Patch has little or no deterioration.

Medium Severity: Patch is moderately deteriorated.

High Severity: Patch is badly deteriorated.

10. Small Patch



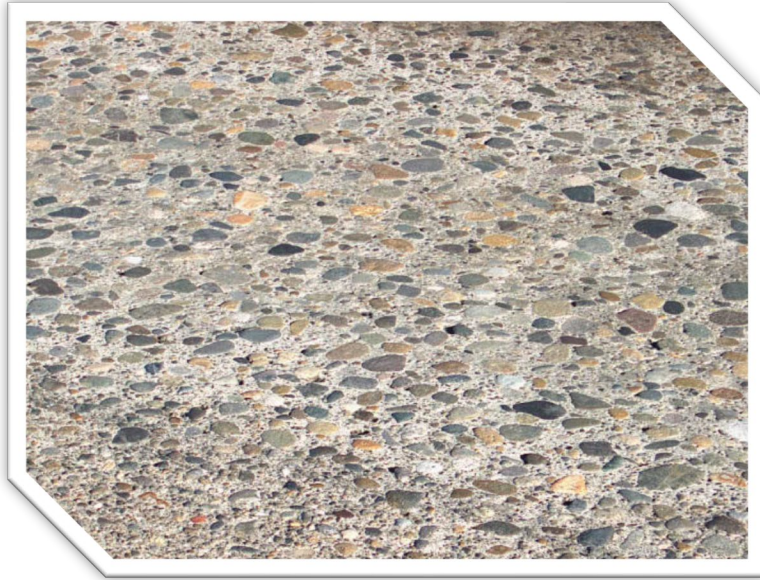
Patch that is smaller than 5.5 sq ft.

Low Severity: Patch has little or no deterioration.

Medium Severity: Patch is moderately deteriorated.

High Severity: Patch is badly deteriorated.

11. Polished Aggregate



Distress where traffic smooths the pavement surface so friction is diminished and cars can slide.

There are no Severity Levels for this distress.

12. Popouts



Small piece of pavement that breaks loose from surface.

There are no Severity Levels for this distress, however popouts must cover 3 per sq. meter of the slab.

13. Pumping



Ejection of material from slab foundation through joints or cracks along with water.

There are no Severity Levels for this distress.

14. Punchout



Localized area of a slab that is broken into many pieces.

Severity is determined by the following matrix.

Severity of Majority of Cracks	Number of Pieces		
	2 to 3	4 to 5	> 5
L	L	L	M
M	L	M	H
H	M	H	H

SECTION III

**PAVEMENT CONDITION INDEX (PCI) REPORTS
(COMMUNITY-WIDE)**

- A. Fire Access Maintenance District (FAMD) 2024 PCI Map
 - B. Name Order (A-Z)
 - C. PCI Order (0-100)

PAVEMENT CONDITION INDEX (PCI) REPORT DEFINITIONS

Listed alphabetically by street name or PCI, this report provides the Community with a listing of pertinent inventory and pavement condition data for each inventory unit within the FAMD pavement network. The Pavement Condition Index (PCI) Report notes the names, limits, classification, dimension, surface type, and lane configuration of each inventory unit.

Detailed descriptions of the information appearing on this report are presented below:

BRANCH NAME - The name of each inventory unit appears in this column. Generally, the inventory unit name is taken directly from a street sign; however, where no street signs are posted, the name appearing on the network map is noted instead. A sample set of street name suffix abbreviation definitions is presented below:

AVE - Avenue	CT - Court	CIR - Circle
DR - Drive	LN - Lane	RD - Road
ST - Street	WY - Way	EB - East Bound
NB - North Bound	SB - South Bound	WB - West Bound
TER - Terrace	PL - Place	

FROM - A description of the beginning limit of each inventory unit appears in this column. If the beginning limit exists between intersections, then the beginning limit description may be an address, post mile marker, or a distance from a known point of reference (e.g., "500' N/MAIN ST").

TO - A description of the ending limit of each inventory unit appears in this column. Like BEGIN limit, the END limit description may consist of a street name, an address, or a distance from a known point of reference. In the case of cul-de-sacs, or dead-ends, the END limit consists of an address, or a directional reference, such as "NORTH END," when no address is available.

STREET CLASSIFICATION - The code for one street classification is represented below. The following code are classified according to FAMD classifications.

<u>CODE</u>	<u>DESCRIPTION</u>
E	Local

SURFACE TYPE - A code was assigned to each inventory unit to describe surface type.

<u>CODE</u>	<u>DESCRIPTION</u>
AC	Asphalt Concrete
PCC	Concrete

LENGTH - The length of the section within each branch.

UNITS - The unit of measurement for the section length, typically linear feet (LF).

AREA - The area of each section within a branch.

UNITS - The unit of measurement for the section area, typically square feet (SF).

PCI - Pavement Condition Indices were calculated for inventory units based on severity and extent of distress manifestations observed within the inventory unit. Ranging between 0 and 100, a PCI of "100" corresponds to a pavement at the beginning of its life cycle, while a PCI of "0" corresponds to a badly deteriorated pavement which is at or near the end of its life cycle.

PCI CLIMATE, LOAD AND OTHER – reflects “Section Extrapolated Distress”; these values are shown within the Sample Distresses tab within the PCI window. Distresses are aggregated based on the type and severity level. For random samples, distress quantities are adjusted to reflect the extrapolated value based on the sections total area. Extrapolated distress deducts are classified as resulting from Climate, Load and Other distresses. The Distress Classification portion of the tab shows the “percent” of extrapolated distress deduct belonging to Climate, Load and Other (these %’s are shown within the PCI reports herein). These values are beneficial in that they support the decision whether recommend slurry seal, overlay or reconstruction project for street sections.

Asphalt Distresses	Cause Classification	PCC Distresses	Cause Classification
Alligator cracking	Load	Blow up	Climate
Bleeding	Other	Corner break	Load
Block cracking	Climate	Divided Slab	Load
Bumps/Sags	Other	Durability cracking	Climate
Corrugation	Other	Faulting	Other
Depression	Other	Joint Seal cracking	Climate
Edge cracking	Load	Lane Shoulder Drop-off	Climate
Joint Reflection cracking	Climate	Linear cracking	Load
Lane Shoulder Drop-off	Climate	Small Patching	Other
L&T cracking	Climate	Large Patching	Other
Patch/Utility cut	Other	Polished Agg	Load
Polished Agg	Other	Popouts	Other
Pothole	Climate	Pumping	Other
RR Crossing	Other	Punchout	Load
Rutting	Load	RR Crossing	Other
Shoving	Other	Scaling/crazing	Other
Slippage cracking	Other	Shrinkage cracking	Other
Swell	Other	Corner Spall	Other
Raveling	Other	Joint Spall	Other
Weathering	Climate		

INSPECTION DATE – Represents the most recent inspection date performed on a given sections. PCI shown is historical in value and may not indicate what “today’s” PCI is due to variance in time. Pavement deterioration calculations can be performed on a section(s) to demonstrate a deteriorated PCI based upon a new current date.

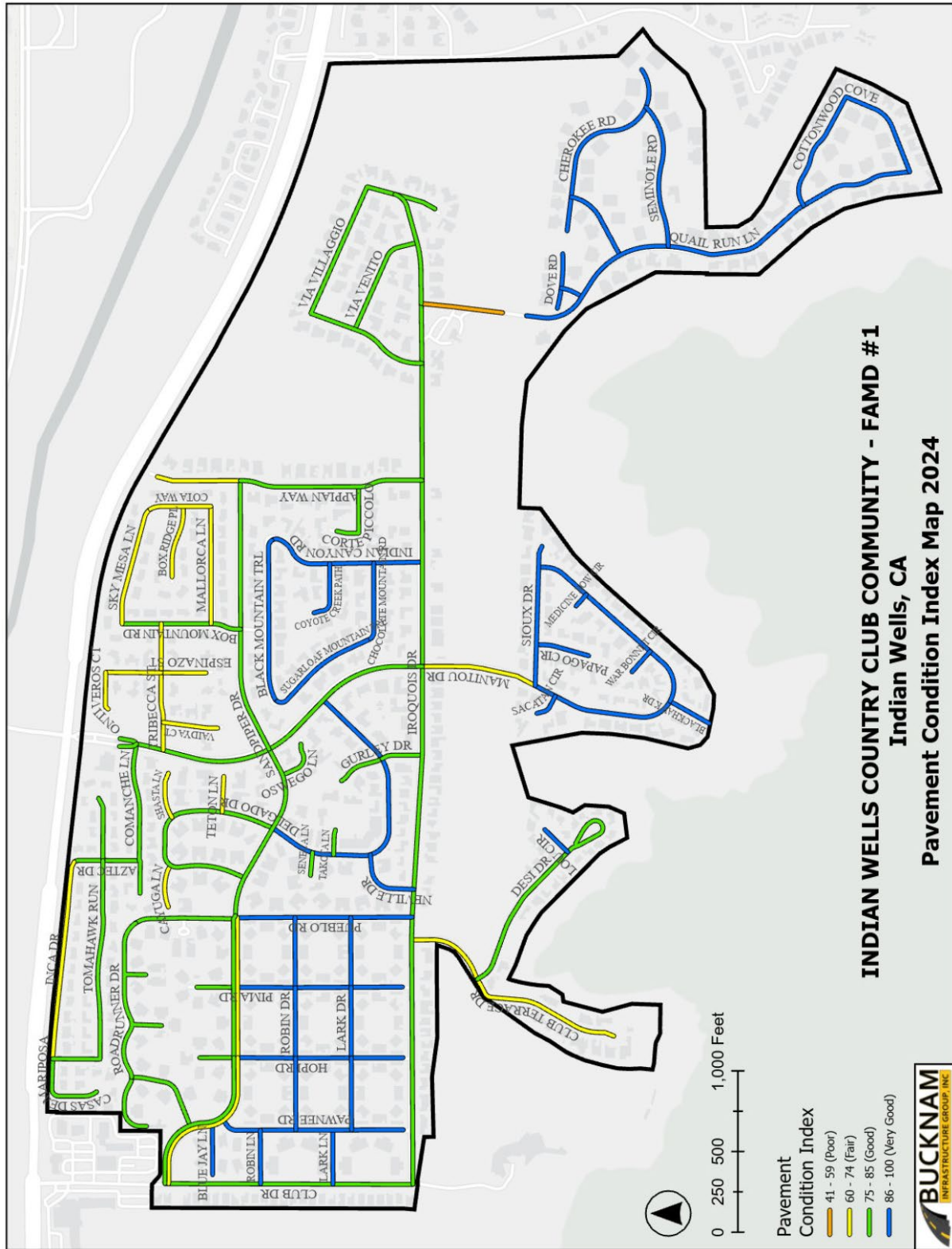


Figure 7 – Fire Access Maintenance District PCI Map - 2024

Indian Wells Country Club - FAMD #1
Pavement Condition Index (PCI) Report - All Streets

BranchID	Sec ID	Name	From	To	Surface Type	Rank	L	W	Area	Insp. Date	PCI	PCI Pct	Climate	PCI Pct	Load	PCI Pct	Other	Work Type	Work Date
1	20	APPIAN WAY	IROQUOIS DR	SANDPIPER DR	AC	E	1,150	36	41,080	6/13/24	77	81		19	0	0	New Construction - Initial	1/1/00	
1	20	APPIAN WAY	SANDPIPER DR	CDS	AC	E	502	32	17,844	6/13/24	73	67		33	0	0	New Construction - Initial	1/1/00	
2	30	AZTEC DR	COMANCHE LN	INCA DR	AC	E	419	35	13,465	6/13/24	79	80		20	0	0	New Construction - Initial	1/1/00	
3	40	BLACK MOUNTAIN TRL	INDIAN CANYON RD	SUGARLOAF MOUNTAIN TRL	AAC	E	1,016	36	38,626	6/14/24	99	100		0	0	0	Overlay - AC Structural	11/1/23	
4	50	BLACKHAWK DR	MANITOU DR	END	AC	E	273	30	8,190	6/13/24	92	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
5	60	BLUE JAY LN	SANDPIPER DR	SKY MESA LN	AAC	E	345	30	10,350	6/14/24	98	100		0	0	0	Surface Reconstruction - AC	11/1/21	
6	70	BOX MOUNTAIN RD	SANDPIPER DR	SKY MESA LN	AC	E	731	39	30,749	6/14/24	79	54		46	0	0	New Construction - Initial	1/1/00	
7	80	BOX RIDGE PL	COTA WAY	CDS	AC	E	448	28	13,824	6/14/24	69	55		45	0	0	New Construction - Initial	1/1/00	
8	90	CASAS DE MARIPOSA	TOMAHAWK RUN	CDS	AC	E	498	32	17,686	6/13/24	83	85		15	0	0	New Construction - Initial	1/1/00	
9	100	CAYUGA LN	DELGADO DR	CDS	AC	E	267	28	8,926	6/14/24	70	60		40	0	0	Surface Treatment - Slurry Seal	11/1/23	
10	110	CHEROKEE RD	QUAIL RUN	CHEROKEE RD	AC	E	489	27	13,203	6/14/24	100	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
10	120	CHEROKEE RD	END	END	AC	E	1,274	27	35,878	6/14/24	94	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
11	130	CHOCOLATE MOUNTAIN RD	SUGARLOAF MOUNTAIN TRL	INDIAN CANYON RD	AAC	E	476	36	17,986	6/14/24	99	100		0	0	0	New Construction - Initial	1/1/00	
12	140	CLUB DR	IROQUOIS DR	ROBIN LN	AC	E	945	45	42,525	6/13/24	78	66		34	0	0	Surface Treatment - Slurry Seal	11/1/23	
12	150	CLUB DR	ROBIN LN	SANDPIPER DR	AC	E	582	45	26,190	6/13/24	80	88		12	0	0	Surface Treatment - Slurry Seal	11/1/23	
13	160	CLUB TERRACE DR	IROQUOIS DR	DESI DR	AC	E	524	28	14,812	6/13/24	64	47		52	1	0	Surface Treatment - Slurry Seal	11/1/23	
13	170	CLUB TERRACE DR	DESI DR	CDS	AC	E	975	30	30,900	6/13/24	73	79		21	0	0	Surface Treatment - Slurry Seal	11/1/23	
14	180	COMANCHE LN	MANITOU DR	CDS	AC	E	918	36	33,048	6/13/24	78	73		27	0	0	New Construction - Initial	2/1/00	
15	190	CORTE PICCOLO	APPIAN WAY	AC	AC	E	435	36	17,510	6/13/24	79	100		0	0	0	New Construction - Initial	2/1/00	
16	200	COTA WAY	SKY MESA LN	MALLORCA LN	AC	E	451	28	13,878	6/14/24	72	61		39	0	0	New Construction - Initial	2/1/00	
17	210	COTTONWOOD COVE	QUAIL RUN LN (N)	QUAIL RUN LN (S)	AAC	E	1,408	30	44,490	6/14/24	100	0		0	0	0	Overlay - AC Structural	11/1/23	
18	220	COVOTE CREEK PATH	INDIAN CANYON RD	CDS	AC	E	370	34	14,500	6/14/24	97	100		0	0	0	Overlay - AC Structural	11/1/22	
19	230	DELGADO DR	MANITOU DR	NEVILLE DR	AAC	E	1,169	28	32,732	6/14/24	100	0		0	0	0	Overlay - AC Structural	11/1/21	
19	240	DELGADO DR	NEVILLE DR	SANDPIPER DR	AAC	E	651	27	17,577	6/14/24	100	0		0	0	0	Overlay - AC Structural	11/1/21	
19	250	DELGADO DR	SANDPIPER DR (E)	SANDPIPER DR (W)	AC	E	1,444	28	40,432	6/14/24	77	59		39	2	0	Surface Treatment - Slurry Seal	11/1/23	
20	260	DESI DR	CLUB TERRACE DR	CDS	AC	E	1,212	28	33,996	6/13/24	82	59		41	0	0	Surface Treatment - Slurry Seal	11/1/23	
21	270	DOVE RD	QUAIL RUN LN	DOVE RD	AAC	E	110	28	3,080	6/14/24	100	0		0	0	0	Overlay - AC Structural	11/1/23	
21	280	DOVE RD	END	END	AAC	E	345	20	6,900	6/14/24	100	0		0	0	0	Overlay - AC Structural	11/1/23	
22	290	ESPINAZO ST	TRIBECA ST	CDS	AC	E	279	32	10,208	6/14/24	71	78		22	0	0	New Construction - Initial	2/1/00	
22	300	ESPINAZO ST	TRIBECA ST	ONTVEROS CT	AC	E	351	32	11,232	6/14/24	72	78		22	0	0	New Construction - Initial	2/1/00	
23	310	GURLEY DR	IROQUOIS DR	DELGADO DR	AC	E	255	28	7,140	6/14/24	80	70		30	0	0	Surface Treatment - Slurry Seal	11/1/22	
23	320	GURLEY DR	DELGADO DR	CDS	AC	E	292	28	9,456	6/14/24	76	58		42	0	0	Surface Treatment - Slurry Seal	11/1/22	
24	330	HOP RD	LARK LN	END	AAC	E	353	34	12,002	6/14/24	98	100		0	0	0	Overlay - AC Structural	11/1/21	
24	340	HOP RD	LARK LN	ROBIN LN	AAC	E	345	34	11,730	6/14/24	98	100		0	0	0	Overlay - AC Structural	11/1/21	
24	350	HOP RD	ROBIN LN	SANDPIPER DR	AAC	E	370	34	12,580	6/14/24	98	100		0	0	0	Overlay - AC Structural	11/1/21	
24	360	HOP RD	SANDPIPER DR	CDS	AC	E	223	30	8,540	6/14/24	77	100		0	0	0	New Construction - Initial	2/1/00	
25	370	INCA DR	AZTEC DR	TOMAHAWK RUN	AC	E	1,245	26	32,370	6/13/24	74	61		39	0	0	New Construction - Initial	2/1/00	
26	380	INDIAN CANYON RD	IROQUOIS DR	BLACK MOUNTAIN TRL	AAC	E	997	36	37,152	6/14/24	99	100		0	0	0	Overlay - AC Structural	11/1/22	
27	390	IROQUOIS DR	CLUB DR	PUEBLO RD	AC	E	1,667	46	76,682	6/13/24	80	85		13	2	0	Surface Treatment - Slurry Seal	11/1/23	
27	400	IROQUOIS DR	PUEBLO RD	MANITOU DR	AC	E	1,545	46	71,070	6/13/24	84	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
27	410	IROQUOIS DR	MANITOU DR	APPIAN WAY	AC	E	1,154	46	53,084	6/13/24	75	88		12	0	0	New Construction - Initial	2/1/00	
27	420	IROQUOIS DR	APPIAN WAY	QUAIL RUN LN	AC	E	1,099	46	50,554	6/13/24	79	63		33	4	0	New Construction - Initial	2/1/00	
27	430	IROQUOIS DR	QUAIL RUN LN	VIA VILLAGGIO	AC	E	717	46	32,982	6/13/24	81	98		0	2	0	Surface Reconstruction - AC	11/1/21	
28	440	LARK LN	PAWNEE RD	END	AAC	E	311	24	7,464	6/14/24	98	100		0	0	0	Surface Reconstruction - AC	11/1/21	
28	450	LARK LN	PAWNEE RD	HOP RD	AAC	E	446	25	11,150	6/14/24	95	29		0	0	71	Surface Reconstruction - AC	11/1/21	
28	460	LARK LN	HOP RD	PIMA RD	AAC	E	437	25	10,925	6/14/24	98	100		0	0	0	Surface Reconstruction - AC	11/1/21	
28	470	LARK LN	PIMA RD	PUEBLO RD	AAC	E	444	25	11,100	6/14/24	98	100		0	0	0	Surface Reconstruction - AC	11/1/21	
29	480	LOU CIR	DESI DR	CDS	AC	E	200	28	6,850	6/13/24	91	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
30	490	MALLORCA LN	COTA WAY	BOX MOUNTAIN LN	AC	E	728	28	21,064	6/14/24	70	57		43	0	0	New Construction - Initial	2/1/00	
31	500	MANITOU DR	SIoux DR	BLACKHAWK DR	AC	E	1,222	32	39,104	6/13/24	91	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
31	510	MANITOU DR	BLACKHAWK DR	SIoux DR	AC	E	940	30	28,200	6/13/24	95	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	
31	520	MANITOU DR	SIoux DR	IROQUOIS DR	AC	E	71	32	22,822	6/13/24	68	71		19	0	0	New Construction - Initial	2/1/00	
31	530	MANITOU DR	IROQUOIS DR	SANDPIPER DR	AC	E	1,131	45	50,895	6/13/24	78	88		12	0	0	New Construction - Initial	2/1/00	
31	540	MANITOU DR	SANDPIPER DR	COMANCHE LN	AC	E	847	45	38,115	6/13/24	76	68		24	8	0	New Construction - Initial	2/1/00	
31	550	MANITOU DR	GATE	COMANCHE LN	AC	E	88	35	3,080	6/13/24	85	100		0	0	0	New Construction - Initial	2/1/00	
31	560	MANITOU DR	GATE	COMANCHE LN	AC	E	88	33	2,904	6/13/24	85	100		0	0	0	New Construction - Initial	2/1/00	
32	570	MEDICINEROW CIR	MANITOU DR	CDS	AC	E	105	31	3,255	6/13/24	90	100		0	0	0	Surface Treatment - Slurry Seal	11/1/23	

Indian Wells Country Club - FAMD #1
Pavement Condition Index (PCI) Report - All Streets

BranchID	Sec ID	Name	From	To	Surface Type	Rank	L	W	Area	Insp. Date	PCI	PCI Pct Climate	PCI Pct Load	PCI Pct Other	Work Type	Work Date
33	580	NEVILLE DR	IROQUOIS DR	DELGADO DR	AAC	E	417	28	11,676	6/14/24	100	0	0	0	Overlay - AC Structural	11/1/21
34	590	ONTIVEROS CT	EPINAZO ST	CDS	AC	E	239	30	9,120	6/14/24	70	70	30	0	New Construction - Initial	2/1/00
35	600	OSWEGO LN	SANDPIPER DR	CDS	AC	E	286	28	9,388	6/14/24	80	100	0	0	Surface Treatment - Slurry Seal	11/1/22
36	610	PAPAGO CIR	SIoux DR	CDS	AC	E	340	31	11,780	6/13/24	91	100	0	0	Surface Treatment - Slurry Seal	11/1/23
37	620	PAWNEER RD	END	SANDPIPER DR	AAC	E	1,186	34	40,324	6/14/24	98	55	0	45	Overlay - AC Structural	11/1/21
37	630	PAWNEER RD	SANDPIPER DR	ROADRUNNER DR	AC	E	544	30	16,320	6/13/24	82	67	33	0	Surface Treatment - Slurry Seal	11/1/23
38	640	PIMA RD	LARK LN	END	AAC	E	352	34	11,968	6/14/24	98	100	0	0	Overlay - AC Structural	11/1/21
38	650	PIMA RD	ROBIN LN	ROBIN LN	AAC	E	345	34	11,730	6/14/24	98	100	0	0	Overlay - AC Structural	11/1/21
38	660	PIMA RD	ROBIN LN	SANDPIPER DR	AAC	E	370	34	12,580	6/14/24	98	100	0	0	Overlay - AC Structural	11/1/21
38	670	PIMA RD	SANDPIPER DR	CDS	AC	E	240	35	10,700	6/14/24	75	91	9	0	New Construction - Initial	2/1/00
39	680	PUEBLO RD	IROQUOIS DR	SANDPIPER DR	AAC	E	1,100	28	30,800	6/13/24	97	100	0	0	Overlay - AC Structural	11/1/21
39	690	PUEBLO RD	SANDPIPER DR	ROADRUNNER DR	AC	E	607	34	20,638	6/13/24	76	67	33	0	Surface Treatment - Slurry Seal	11/1/23
40	700	QUAIL RUN LN	IROQUOIS DR	BRIDGE	AC	E	488	36	17,318	6/14/24	55	75	24	1	New Construction - Initial	2/1/00
40	710	QUAIL RUN LN	BRIDGE	SEMINOLE RD	AAC	E	1,080	35	37,800	6/14/24	100	0	0	0	Overlay - AC Structural	11/1/23
40	720	QUAIL RUN LN	SEMINOLE RD	COTTONWOOD COVE	AAC	E	1,647	32	50,704	6/14/24	100	0	0	0	Overlay - AC Structural	11/1/23
41	730	ROADRUNNER DR	PUEBLO RD	PAWNEE RD	AC	E	1,024	30	30,720	6/13/24	77	85	0	15	Surface Treatment - Slurry Seal	11/1/23
41	740	ROADRUNNER DR	PAWNEE RD	END	AC	E	406	30	13,680	6/13/24	77	90	10	0	Surface Treatment - Slurry Seal	11/1/23
42	750	ROBIN LN	PAWNEE RD	END	AAC	E	380	23	8,740	6/14/24	98	100	0	0	Surface Reconstruction - AC	11/1/21
42	760	ROBIN LN	PAWNEE RD	HOPi RD	AAC	E	446	25	11,150	6/14/24	98	100	0	0	Surface Reconstruction - AC	11/1/21
42	770	ROBIN LN	HOPi RD	PIMA RD	AAC	E	436	25	10,900	6/14/24	98	100	0	0	Surface Reconstruction - AC	11/1/21
42	780	ROBIN LN	PIMA RD	PUEBLO RD	AAC	E	446	25	11,150	6/14/24	98	100	0	0	Surface Reconstruction - AC	11/1/21
43	790	SACATAN CIR	MANITOU DR	CDS	AC	E	168	31	6,858	6/13/24	96	100	0	0	Surface Treatment - Slurry Seal	11/1/23
44	800	SANDPIPER DR	CLUB DR	HOPi RD	AC	E	1,023	28	28,644	6/13/24	73	58	42	0	Surface Treatment - Slurry Seal	11/1/23
44	810	SANDPIPER DR	HOPi RD	PUEBLO RD	AC	E	860	28	24,080	6/13/24	74	65	35	0	Surface Treatment - Slurry Seal	11/1/23
44	820	SANDPIPER DR	PUEBLO RD	MANITOU DR	AC	E	1,148	45	51,660	6/13/24	79	86	11	3	Surface Treatment - Slurry Seal	11/1/23
44	830	SANDPIPER DR	MANITOU DR	BOX MOUNTAIN RD	AC	E	772	45	34,740	6/13/24	76	92	8	0	New Construction - Initial	2/1/00
44	840	SANDPIPER DR	BOX MOUNTAIN RD	APPiAN WAY	AC	E	930	45	41,950	6/13/24	77	80	20	0	New Construction - Initial	2/1/00
44	850	SANDPIPER DR	PUEBLO RD	HOPi RD	AC	E	860	22	18,920	6/14/24	81	100	0	0	New Construction - Initial	2/1/00
44	860	SANDPIPER DR	HOPi RD	CLUB DR	AC	E	1,023	22	22,506	6/14/24	80	97	0	3	New Construction - Initial	2/1/00
45	870	SEMINOLE RD	QUAIL RUN LN	CHEROKEE RD	AC	E	900	28	25,200	6/14/24	100	0	0	0	Surface Treatment - Slurry Seal	11/1/23
46	880	SENECA LN	DELGADO DR	CDS	AC	E	342	28	5,426	6/14/24	81	97	0	3	Surface Treatment - Slurry Seal	11/1/23
47	890	SHASTA LN	DELGADO DR	CDS	AC	E	304	28	9,962	6/14/24	74	85	15	0	Surface Treatment - Slurry Seal	11/1/23
48	900	SIoux DR	MANITOU DR	CDS	AC	E	882	31	27,942	6/13/24	92	100	0	0	Surface Treatment - Slurry Seal	11/1/23
49	910	SKY MESA LN	BOX MOUNTAIN RD	COTA WAY	AC	E	673	26	17,758	6/14/24	66	53	47	0	New Construction - Initial	2/1/00
50	920	SUGARLOAF MOUNTAIN TRL	BLACK MOUNTAIN TRL	CHOCOLATE MOUNTAIN RD	AAC	E	716	36	27,881	6/14/24	99	100	0	0	Overlay - AC Thin	11/1/22
51	930	TAKOTA LN	DELGADO DR	CDS	AC	E	169	28	5,982	6/14/24	75	79	11	10	Surface Treatment - Slurry Seal	11/1/22
52	940	TETON LN	DELGADO DR	CDS	AC	E	221	28	7,538	6/14/24	71	81	15	4	Surface Treatment - Slurry Seal	11/1/23
53	950	TOMO HAWK RUN	INCA DR	AZTEC DR	AC	E	1,541	32	49,312	6/13/24	82	86	14	0	New Construction - Initial	2/1/00
53	960	TOMO HAWK RUN	AZTEC DR	CDS	AC	E	400	32	13,400	6/13/24	77	100	0	0	New Construction - Initial	2/1/00
54	970	TRIBECCA ST	BOX MOUNTAIN RD	MANITOU DR	AC	E	786	32	24,502	6/14/24	68	59	41	0	New Construction - Initial	2/1/00
55	980	VAIDYA CT	TRIBECCA ST	CDS	AC	E	331	31	11,941	6/14/24	71	83	17	0	New Construction - Initial	2/1/00
56	990	VIA VENITO	VIA VILLAGGIO	IROQUOIS DR	AC	E	706	36	25,416	6/13/24	80	86	14	0	New Construction - Initial	2/1/00
57	1000	VIA VILLAGGIO	IROQUOIS DR (W)	IROQUOIS DR (E)	AC	E	1,809	36	66,104	6/13/24	75	78	19	3	New Construction - Initial	2/1/00
57	1010	VIA VILLAGGIO	IROQUOIS DR	CDS	AC	E	285	35	11,735	6/13/24	79	84	16	0	New Construction - Initial	2/1/00
58	1020	WAR BONNET CIR	MANITOU DR	CDS	AC	E	180	31	6,830	6/13/24	91	100	0	0	Surface Treatment - Slurry Seal	11/1/23
							12.7		2,288,860							

Indian Wells Country Club - FAMD #1
Pavement Condition Index (PCI) Report - All Streets

BranchID	Sec ID	Name	From	To	Surface Type	Rank	L	W	Area	Insp. Date	PCI	PCI Pct Climate	PCI Pct Load	PCI Pct Other	Work Type	Work Date
40	700	QUAIL RUN LN	IROQUOIS DR	BRIDGE	AC	E	488	36	17,318	6/14/24	55	75	24	1	New Construction - Initial	2/1/00
13	160	CLUB TERRACE DR	IROQUOIS DR	DESI DR	AC	E	524	28	14,812	6/13/24	64	47	52	1	Surface Treatment - Slurry Seal	11/1/23
49	510	SKY MESA LN	BOX MOUNTAIN RD	COTA WAY	AC	E	673	26	17,758	6/14/24	66	53	47	0	New Construction - Initial	2/1/00
31	520	MANITOU DR	SIoux DR	IROQUOIS DR	AC	E	721	32	22,822	6/13/24	68	71	29	0	New Construction - Initial	2/1/00
54	970	TRIBECCA ST	BOX MOUNTAIN RD	MANITOU DR	AC	E	786	32	24,502	6/14/24	68	59	41	0	New Construction - Initial	2/1/00
7	80	BOX RIDGE PL	COTA WAY	CDS	AC	E	448	28	13,824	6/14/24	69	55	45	0	New Construction - Initial	1/1/00
34	590	ONTVEROS CT	EPINAZO ST	CDS	AC	E	239	30	9,120	6/14/24	70	70	30	0	New Construction - Initial	2/1/00
9	100	CAYUGA LN	DELGADO DR	CDS	AC	E	267	28	8,926	6/14/24	70	60	40	0	Surface Treatment - Slurry Seal	11/1/23
30	490	MALLOCCA LN	COTA WAY	BOX MOUNTAIN LN	AC	E	728	28	21,064	6/14/24	70	57	43	0	New Construction - Initial	2/1/00
22	290	EPINAZO ST	TRIBECCA ST	CDS	AC	E	279	32	10,208	6/14/24	71	78	22	0	New Construction - Initial	2/1/00
52	940	TETON LN	DELGADO DR	CDS	AC	E	221	28	7,538	6/14/24	71	81	15	4	Surface Treatment - Slurry Seal	11/1/23
55	980	VAIDYA CT	TRIBECCA ST	CDS	AC	E	331	31	11,941	6/14/24	71	83	17	0	New Construction - Initial	2/1/00
22	300	EPINAZO ST	TRIBECCA ST	ONTVEROS CT	AC	E	351	32	11,232	6/14/24	72	78	22	0	New Construction - Initial	2/1/00
16	200	COTA WAY	SKY MESA LN	MALLOCCA LN	AC	E	451	28	13,878	6/14/24	72	61	39	0	New Construction - Initial	2/1/00
1	20	APPIAN WAY	SANDPIPER DR	CDS	AC	E	502	32	17,844	6/13/24	73	67	33	0	New Construction - Initial	1/1/00
44	800	SANDPIPER DR	CLUB DR	HOPIDR	AC	E	1,023	28	28,644	6/13/24	73	58	42	0	Surface Treatment - Slurry Seal	11/1/23
13	170	CLUB TERRACE DR	DESI DR	CDS	AC	E	975	30	30,900	6/13/24	73	79	21	0	Surface Treatment - Slurry Seal	11/1/23
25	370	INCA DR	AZTEC DR	TOMAHAWK RUN	AC	E	1,245	26	32,270	6/13/24	74	61	39	0	New Construction - Initial	2/1/00
47	890	SHASTA LN	DELGADO DR	CDS	AC	E	304	28	9,962	6/14/24	74	85	15	0	Surface Treatment - Slurry Seal	11/1/23
44	810	SANDPIPER DR	HOPIDR	PUEBLO RD	AC	E	860	28	24,080	6/13/24	74	65	35	0	Surface Treatment - Slurry Seal	11/1/23
27	410	IROQUOIS DR	MANITOU DR	APPIAN WAY	AC	E	1,154	46	53,084	6/13/24	75	88	12	0	New Construction - Initial	2/1/00
38	670	PIMA RD	SANDPIPER DR	CDS	AC	E	240	35	10,700	6/14/24	75	91	9	0	New Construction - Initial	2/1/00
51	930	TAKOTA LN	DELGADO DR	CDS	AC	E	169	28	5,982	6/14/24	75	65	35	0	New Construction - Initial	11/1/22
57	1000	VIA VILLAGGIO	IROQUOIS DR (W)	IROQUOIS DR (E)	AC	E	1,809	36	66,104	6/13/24	75	78	19	3	Surface Treatment - Slurry Seal	2/1/00
39	690	PUEBLO RD	SANDPIPER DR	ROADRUNNER DR	AC	E	607	34	20,638	6/13/24	76	67	33	0	Surface Treatment - Slurry Seal	11/1/23
31	540	MANITOU DR	SANDPIPER DR	COMANCHE LN	AC	E	847	45	38,115	6/13/24	76	68	24	8	New Construction - Initial	2/1/00
23	320	GURLEY DR	DELGADO DR	CDS	AC	E	292	28	9,456	6/14/24	76	58	42	0	Surface Treatment - Slurry Seal	11/1/22
44	730	SANDPIPER DR	MANITOU DR	BOX MOUNTAIN RD	AC	E	772	45	34,740	6/13/24	76	92	8	0	New Construction - Initial	2/1/00
41	830	ROADRUNNER DR	PUEBLO RD	PAWNEE RD	AC	E	1,024	30	30,720	6/13/24	77	85	15	0	Surface Treatment - Slurry Seal	11/1/23
41	740	ROADRUNNER DR	PAWNEE RD	END	AC	E	406	30	13,680	6/13/24	77	90	10	0	Surface Treatment - Slurry Seal	11/1/23
44	840	SANDPIPER DR	BOX MOUNTAIN RD	APPIAN WAY	AC	E	930	45	41,850	6/13/24	77	80	20	0	New Construction - Initial	2/1/00
1	10	APPIAN WAY	IROQUOIS DR	SANDPIPER DR	AC	E	1,150	36	41,080	6/13/24	77	81	19	0	New Construction - Initial	1/1/00
24	360	HOPIDR	SANDPIPER DR	CDS	AC	E	223	30	8,540	6/14/24	77	100	0	0	New Construction - Initial	2/1/00
19	250	DELGADO DR	SANDPIPER DR (E)	SANDPIPER DR (W)	AC	E	1,444	28	40,432	6/14/24	77	59	39	2	Surface Treatment - Slurry Seal	2/1/00
53	960	TOMAHAWK RUN	AZTEC DR	CDS	AC	E	400	32	13,400	6/13/24	77	100	0	0	New Construction - Initial	2/1/00
14	180	COMANCHE LN	MANITOU DR	CDS	AC	E	918	36	33,048	6/13/24	78	73	27	0	New Construction - Initial	2/1/00
31	530	MANITOU DR	IROQUOIS DR	SANDPIPER DR	AC	E	1,131	45	50,895	6/13/24	78	88	12	0	New Construction - Initial	2/1/00
12	140	CLUB DR	IROQUOIS DR	ROBIN LN	AC	E	945	45	42,525	6/13/24	78	66	34	0	Surface Treatment - Slurry Seal	11/1/23
44	820	SANDPIPER DR	PUEBLO RD	MANITOU DR	AC	E	1,148	45	51,660	6/13/24	79	86	11	3	Surface Treatment - Slurry Seal	11/1/23
6	70	BOX MOUNTAIN RD	SANDPIPER DR	SKY MESA LN	AC	E	435	39	30,749	6/14/24	79	54	46	0	New Construction - Initial	1/1/00
57	1010	VIA VILLAGGIO	IROQUOIS DR	CDS	AC	E	285	35	11,735	6/13/24	79	84	16	0	New Construction - Initial	2/1/00
2	30	AZTEC DR	COMANCHE LN	INCA DR	AC	E	419	35	13,465	6/13/24	79	80	20	0	New Construction - Initial	1/1/00
15	190	CORTE PICCOLO	APPIAN WAY	CDS	AC	E	435	36	17,510	6/13/24	79	100	0	0	New Construction - Initial	2/1/00
27	420	IROQUOIS DR	APPIAN WAY	QUAIL RUN LN	AC	E	1,099	46	50,554	6/13/24	79	63	33	4	New Construction - Initial	2/1/00
27	390	IROQUOIS DR	CLUB DR	PUEBLO RD	AC	E	1,667	46	76,682	6/13/24	80	85	13	2	Surface Treatment - Slurry Seal	11/1/23
35	600	OSWEGO LN	SANDPIPER DR	IROQUOIS DR	AC	E	286	28	9,388	6/14/24	80	100	0	0	Surface Treatment - Slurry Seal	2/1/00
56	990	VIA VENITO	VIA VILLAGGIO	IROQUOIS DR	AC	E	706	36	25,416	6/13/24	80	86	14	0	New Construction - Initial	2/1/00
12	150	CLUB DR	ROBIN LN	SANDPIPER DR	AC	E	582	45	26,190	6/13/24	80	88	12	0	Surface Treatment - Slurry Seal	11/1/23
44	860	SANDPIPER DR	HOPIDR	CLUB DR	AC	E	1,023	22	22,506	6/14/24	80	97	0	3	New Construction - Initial	2/1/00
23	310	GURLEY DR	IROQUOIS DR	DELGADO DR	AC	E	255	28	7,140	6/14/24	80	70	30	0	Surface Treatment - Slurry Seal	11/1/22
44	850	SANDPIPER DR	PUEBLO RD	HOPIDR	AC	E	860	22	18,920	6/14/24	81	100	0	0	New Construction - Initial	2/1/00
46	880	SENECA LN	DELGADO DR	CDS	AC	E	142	28	5,426	6/14/24	81	98	0	3	Surface Treatment - Slurry Seal	2/1/00
27	430	IROQUOIS DR	QUAIL RUN LN	VIA VILLAGGIO	AC	E	717	46	32,982	6/13/24	81	97	0	2	New Construction - Initial	11/1/23
37	630	PAMNEER DR	SANDPIPER DR	ROADRUNNER DR	AC	E	544	30	16,320	6/13/24	82	67	33	0	Surface Treatment - Slurry Seal	11/1/23
20	260	DESI DR	CLUB TERRACE DR	CDS	AC	E	1,212	28	33,996	6/13/24	82	59	41	0	Surface Treatment - Slurry Seal	11/1/23
53	950	TOMAHAWK RUN	INCA DR	AZTEC DR	AC	E	1,541	32	49,312	6/13/24	82	86	14	0	New Construction - Initial	2/1/00
8	90	CASAS DE MARIPOSA	TOMAHAWK RUN	CDS	AC	E	498	32	17,686	6/13/24	83	85	15	0	New Construction - Initial	1/1/00

Indian Wells Country Club - FAMD #1
Pavement Condition Index (PCI) Report - All Streets

BranchID	Sec ID	Name	From	To	Surface Type	Rank	L	W	Area	Insp. Date	PCI	PCI Pct	Climate	PCI Pct	Load	PCI Pct	Other	Work Type	Work Date
27	400	IROQUOIS DR	PUEBLO RD	MANITOU DR	AC	E	1,545	46	71,070	6/13/24	84	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
31	550	MANITOU DR	COMANCHE LN	GATE	AC	E	88	35	3,080	6/13/24	85	100	100	0	0	0	0	New Construction - Initial	2/1/00
31	560	MANITOU DR	GATE	COMANCHE LN	AC	E	88	33	2,904	6/13/24	85	100	100	0	0	0	0	New Construction - Initial	2/1/00
32	570	MEDICINE ROW CIR	MANITOU DR	CDS	AC	E	105	31	3,255	6/13/24	90	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
36	610	PAPAGO CIR	SIoux DR	CDS	AC	E	340	31	11,780	6/13/24	91	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
31	500	MANITOU DR	SIoux DR	BLACKHAWK DR	AC	E	1,222	32	39,104	6/13/24	91	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
58	1020	WAR BONNET CIR	MANITOU DR	CDS	AC	E	180	31	6,830	6/13/24	91	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
29	480	LOU CIR	DESI DR	CDS	AC	E	200	28	6,850	6/13/24	91	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
4	50	BLACKHAWK DR	MANITOU DR	END	AC	E	273	30	8,190	6/13/24	92	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
48	900	SIoux DR	MANITOU DR	CDS	AC	E	882	31	27,342	6/13/24	92	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
10	120	CHEROKEE RD	END	END	AC	E	1,274	27	35,878	6/14/24	94	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
31	510	MANITOU DR	BLACKHAWK DR	SIoux DR	AC	E	940	30	28,700	6/13/24	95	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
28	450	LARK LN	PAWNEE RD	HOPi RD	AC	E	446	25	11,150	6/14/24	95	29	100	0	0	71	0	Surface Reconstruction - AC	11/1/21
43	790	SACATAN CIR	MANITOU DR	CDS	AC	E	168	31	6,858	6/13/24	96	100	100	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
39	680	PUEBLO RD	IROQUOIS DR	SANDPIPER DR	AC	E	1,100	28	30,800	6/13/24	97	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
18	220	COYTE CREEK PATH	INDIAN CANYON RD	CDS	AC	E	370	34	14,500	6/14/24	97	100	100	0	0	0	0	Overlay - AC Structural	11/1/22
28	440	LARK LN	PAWNEE RD	END	AC	E	311	24	7,464	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
28	470	LARK LN	PIMA RD	PUEBLO RD	AC	E	444	25	11,100	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
28	460	LARK LN	HOPi RD	PIMA RD	AC	E	437	25	10,925	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
37	620	PAWNEE RD	END	SANDPIPER DR	AC	E	1,186	34	40,324	6/14/24	98	55	100	0	0	45	0	Overlay - AC Structural	11/1/21
42	760	ROBIN LN	PAWNEE RD	HOPi RD	AC	E	446	25	11,150	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
24	340	HOPi RD	LARK LN	ROBIN LN	AC	E	345	34	11,730	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
5	60	BLUE JAY LN	SANDPIPER DR	END	AC	E	345	30	10,350	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
42	750	ROBIN LN	PAWNEE RD	END	AC	E	380	23	8,740	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
24	330	HOPi RD	LARK LN	END	AC	E	353	34	12,002	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
24	350	HOPi RD	ROBIN LN	SANDPIPER DR	AC	E	370	34	12,580	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
38	660	PIMA RD	ROBIN LN	SANDPIPER DR	AC	E	370	34	12,580	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
38	640	PIMA RD	LARK LN	END	AC	E	352	34	11,968	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
38	650	PIMA RD	LARK LN	ROBIN LN	AC	E	345	34	11,730	6/14/24	98	100	100	0	0	0	0	Overlay - AC Structural	11/1/21
42	770	ROBIN LN	HOPi RD	PIMA RD	AC	E	436	25	10,900	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
42	780	ROBIN LN	PUEBLO RD	PIMA RD	AC	E	446	25	11,150	6/14/24	98	100	100	0	0	0	0	Surface Reconstruction - AC	11/1/21
11	130	CHOCOLATE MOUNTAIN TRL	SUGARLOAF MOUNTAIN TRL	INDIAN CANYON RD	AC	E	476	36	17,986	6/14/24	99	100	100	0	0	0	0	New Construction - Initial	1/1/00
26	380	INDIAN CANYON RD	IROQUOIS DR	BLACK MOUNTAIN TRL	AC	E	997	36	37,152	6/14/24	99	100	100	0	0	0	0	Overlay - AC Structural	11/1/22
3	40	BLACK MOUNTAIN TRL	INDIAN CANYON RD	SUGARLOAF MOUNTAIN TRL	AC	E	1,016	36	38,626	6/14/24	99	100	100	0	0	0	0	Overlay - AC Structural	11/1/22
50	920	SUGARLOAF MOUNTAIN TRL	BLACK MOUNTAIN TRL	CHOCOLATE MOUNTAIN RD	AC	E	716	36	27,881	6/14/24	100	0	0	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
10	110	CHEROKEE RD	QUAIL RUN	CHEROKEE RD	AC	E	489	27	13,203	6/14/24	100	0	0	0	0	0	0	Overlay - AC Thin	11/1/23
17	210	COTTONWOOD COVE	QUAIL RUN LN (N)	QUAIL RUN LN (S)	AC	E	1,408	30	44,490	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
19	230	DELGADO DR	MANITOU DR	NEVILLE DR	AC	E	1,169	28	32,732	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
19	240	DELGADO DR	NEVILLE DR	SANDPIPER DR	AC	E	651	27	17,577	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/21
21	280	DOVE RD	END	END	AC	E	345	20	6,900	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
21	270	DOVE RD	QUAIL RUN LN	DOVE RD	AC	E	110	28	3,080	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
33	580	NEVILLE DR	IROQUOIS DR	DELGADO DR	AC	E	417	28	11,676	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/21
40	710	QUAIL RUN LN	BRIDGE	SEMINOLE RD	AC	E	1,080	35	37,800	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
40	720	QUAIL RUN LN	SEMINOLE RD	COTTONWOOD COVE	AC	E	1,647	32	50,704	6/14/24	100	0	0	0	0	0	0	Overlay - AC Structural	11/1/23
45	870	SEMINOLE RD	QUAIL RUN LN	CHEROKEE RD	AC	E	900	28	25,200	6/14/24	100	0	0	0	0	0	0	Surface Treatment - Slurry Seal	11/1/23
							12.7		2,288,860										

MEMO

Date: May 4, 2026

To: Tony and Scott

From: Steve Nozet

Re: Landscape committee

Follows are my notes from our meeting April 30,2026

The three of us met along with the representative from our landscape company.

We walked the grounds of both the Manitou gate and Club Drive entrances

Generally speaking we found the areas in question to be in need of a great deal of service and perhaps even some dramatic change.

All about the Manitou gate there is an abundance of not natural to the desert plants and trees that require a great deal of service and in fact maybe extensive replacement. This does not mean criticism of our landscape company rather a testimony of how fast these plants and trees grow.

Which poses the key question facing the FAMD board:

Do we continue to support this same number of unnatural plants and trees or do we start and complete the process of switching to a more desert landscape?

I would say the issue is of greater concern at Manitou as Club Drive appears to currently appear more naturally landscaped.

I think you Tony and I should meet first to spend more time detailing the needs and or changes of each the areas around the building perhaps with picture to better present to the entire board a more complete view of the situation.

Page two

Landscape committee notes

The fountains.

I will start by pointing out the similar key question as it pertains to the fountains with two separate issues that are different at each of the gates.

Club Drive

The fountain is currently not operating. The issue of why not wasn't made clear and an investigation should be done to determine what's wrong and how to fix it.

Here we go; According to Scott, the basic design of the fountain is that two things happen, water splashes excessively and for some reason the fountain ceases to operate often.

So here we go 1. yes, we need to get a quote for the immediate repair or perhaps just get it running again. But what if we don't, given it is summer season not near as much traffic. 2. Get a comprehensive assessment of the fountain to determine if we repair it, fix the design and, or replace it with a new fountain of a better design or no fountain and a different center island approach.

Manitou Gate

This fountain seems to be of a better esthetic and function design, but it too suffers from need for frequent service of the pump and filter system

The decision of action to take here is not as urgent as Club Drive but raises the same question: Fountains yes or no.

I suspect that either conclusion will be far more expensive than we could imagine.

Fellows, please accept the notes as I've best recalled our meeting and investigations. I present it as I remember and as I see the situation.

I think Tony and I should meet again, try and be more specific as to what immediate action should be taken, cut trim, plant (color) and start down the path of a long term plan

Respectfully submitted

Steve Nozet